



Sonoma County Design Review Committee STAFF REPORT

FILE: PLP19-0045
DATE: April 7, 2021
TIME: At or after 1:30 pm
STAFF: Katrina Braehmer, Project Planner

SUMMARY

Property Owner: Alisha and Natasha Properties, LLC
Applicant: Ken Di Lillo
Address: 685 Aviation Boulevard, Santa Rosa
Supervisorial District(s): Fourth
APN: 059-340-056
Description: Request for Design Review and a Use Permit for a new 59,066 square foot, 50-foot tall, two-story warehouse, with 500+/- square feet to be used as office space, on a 1.53-acre parcel located in the Airport Business Center. The warehouse is a permitted use, and a Use Permit is required for additional building height above the 28-foot maximum within the building setback line.
CEQA Review: Mitigated Negative Declaration
General Plan Land Use: Limited Industrial
Specific/Area Plan Land Use: Industrial Park
Ordinance Reference: Article 44 – Industrial Park District
Article 82 – Design Review
Article 86 – Parking Regulations
Zoning: MP (Industrial Park) 1 AC AVG (One-acre average), VOH (Valley Oak Habitat)



RECOMMENDATION

Staff recommends that the Design Review Committee grant final Design Review and recommend that Permit Sonoma adopt the Mitigated Negative Declaration administratively, subject to and upon Use Permit approval.

EXECUTIVE SUMMARY

Ken Di Lillo, on behalf of NeilMed Pharmaceuticals, requests final Design Review of a new 59,066 square foot, 50-foot tall, two-story warehouse for dry-goods on a 1.53-acre parcel located in the Airport Business Center. The warehouse will provide two loading docks and 500 square feet office space. Other site improvements include paved parking and circulation areas, landscaping, and site lighting. The warehouse is a permitted use, and a Use Permit is required for additional building height above the 28-foot maximum within the 40-foot average building setback line established by the Airport Industrial Area Specific Plan.

One member of the public has submitted a comment supporting the project. No other public comment has been received to date. An Initial Study and Mitigated Negative Declaration were prepared pursuant to the California Environmental Quality Act (CEQA). It has been determined that there is no substantial evidence that the project as approved will have a significant environmental effect because mitigation measures and a mitigation monitoring program have been incorporated into the project as Conditions of Approval and agreed upon by the project applicant.

PROJECT SITE AND CONTEXT

Background

The site is mostly undeveloped, except for a paved parking area along the southern edge of the parcel and utilized by the neighboring parcel through easement. The site and the parcel to the south were originally a single lot, prior to its subdivision in 1998. The minor subdivision was approved jointly with a use permit for planned development status to allow for future construction of two industrial buildings, one on each lot. Separate design review applications were approved for both buildings, though only the southern parcel was developed.

The Specific Plan is in the process of being updated, and no zoning or land use changes are proposed for the project site or abutting parcels.

NeilMed Pharmaceuticals also owns the parcel to the northeast (APN 059-271-044), which comprises office and warehouse space, and the undeveloped parcel directly east (APN 059-430-007).

Site Characteristics

The square-shaped, generally flat site is surrounded on three sides by development and approximately 110 feet above sea level. Existing vegetation is primarily disturbed California annual grassland.

Area Context and Surrounding Land Uses

Direction	Land Uses
North	Light manufacturing and warehousing
South	Office



East	Warehousing and office
West	Office

Significant Applications Nearby

The Hyatt Place Sonoma Wine Country project at 3750 North Laughlin Road (Permit Sonoma file no. PLP19-0009) is a 165-room hotel with a 175-seat restaurant near the intersection of Skylane Boulevard and North Laughlin Road. The application is still in process.

A 28,000-square foot warehouse for wine storage and distribution is proposed for 5496 Skylane Boulevard (Permit Sonoma file no. PLP19-0037), at the southwest corner of Skylane and Aviation Boulevard. The warehouse has received final design review approval from the Design Review Committee, and is undergoing environmental review. This application also includes a request to exceed the Airport Industrial Area Specific Plan height limit, subject to Use Permit approval.

Access

Access is secured from Aviation Boulevard by easement through the property to the south and the private driveway to the east that serves 601 Aviation Boulevard, owned by NeilMed. Truck ingress and egress will be from the private driveway only.

Water/Wastewater/Utilities

Water supply will be provided by the Town of Windsor and the warehouse will connect to the Airport/Larkfield/Wikiup Sanitation District (Sonoma Water) for domestic wastewater service. The warehouse will not contain drains and there will be no industrial wastewater.

Other Environmental Conditions

The project is within the regulatory Santa Rosa Plain. The Programmatic Biological Opinion for the Santa Rosa Plain Conservation Strategy lists the site as “May affect listed plants and would like affect CTS.”

PROJECT DESCRIPTION

Ken Di Lillo proposes to develop a new two-story, 59,066 square foot warehouse for dry-goods, with two truck loading docks on a 1.53-acre parcel. Other site improvements include paved parking and circulation areas, landscaping, and site lighting. The purpose of the project is to supplement an existing warehouse on an adjacent property under the same ownership. Twenty employees are anticipated for warehouse operations, which will occur on weekdays from 5:00 am to 12:00 am.

The 50-foot tall warehouse is proposed to have precast concrete panels for the exterior walls, with the east elevation comprising the central entry and a second-story observation balcony. The two stories are served by two stairwells, a freight elevator, and a passenger elevator. The recessed loading docks will be on the north side of the building, and three other surface level loading doors are proposed. The final parking configuration will include 43 spaces onsite, with 14 dedicated to the office building south of the site. Two stalls will have electric vehicle charging stations, three will be reserved for clean air or vanpool vehicles, and two will be ADA-compliant. Landscaping will be installed at the western and northern perimeter of the parcel and along the eastern and southern edges of the building.



Project History

The table below summarizes key project milestones and events.

Date	Project Event/Milestone
4/16/2019	Application submittal
10/16/2019	Completeness (excluding technical analyses)
10/21/2019	Referral to prominent agencies
12/18/2019	DRC preliminary review
2/24/2021 – 3/26/2021	Initial Study and Mitigated Negative Declaration Public Review Period
3/26/2021	Notice of Final Design Review Meeting
4/1/2021	Notice of Use Permit hearing waiver
4/27/2021	End of hearing waiver noticing period

Prior Review

The table below summarizes prior actions undertaken for the Project.

Date	Authority	Action Taken
12/18/2019	DRC	Recommendations provided related to the site plan, parking, and architecture.

General Plan and Area Plans

The following General Plan and Specific Plan policies are applicable to the project:

LAND USE ELEMENT

The General Plan Land Use Element does not contain any specific policies applicable to the proposed project. However, the overall purpose and definition for the Limited Industrial land use category is to provide sites for development to meet service and employment needs where the range or scale of industrial uses is limited.

OPEN SPACE AND RESOURCE CONSERVATION ELEMENT

Policy OSRC-5b: Use the following general urban design principles until Urban Design Guidelines specific to each Urban Service Area are adopted.

- (1) Promotion of pedestrian and/or bicycle use.
- (2) Compatibility with adjacent development.
- (3) Incorporation of important historical and natural resources.
- (4) Complementary parking out of view of the streetscape.
- (5) Opportunities for social interaction with other community members.
- (6) Promotion of visible access to buildings and use areas.
- (7) Appropriate lighting levels.

The Airport Industrial Area Specific Plan does not contain specific policies applicable to the proposed project.



Zoning

The table below summarizes the development standards that apply to the site as outlined in the Airport Industrial Area Specific Plan and Sonoma County Zoning Ordinance, the proposed development, and whether the project is consistent with the standards.

Standard	Airport Industrial Area Specific Plan	Zoning Ordinance	Project	Standards Met
Front Yard	25 ft. minimum 40 ft. average	n/a	25 ft. minimum 71 ft. average	Yes
Side Yard	10 ft.	n/a	35 ft. (minimum)	Yes
Rear Yard	10 ft.	10 ft.	48 ft. (minimum)	Yes
Height	28 ft. maximum between the 25-foot minimum setback and 40-foot average	65 ft.	50 ft.	Yes
Lot Coverage	50%	50%	44.3%	Yes
Landscaping	20%	20%	20.1%	Yes
Parking Spaces	n/a	32	43 onsite 29 reserved	Yes; see analysis section.

ANALYSIS

Consistency with the General Plan and Specific Plan

The project meets the purpose of the General Plan’s Limited Industrial land use designation because additional light industrial space will help meet the service and employment needs of the County. The project is consistent with the development intensity criteria of the Limited Industrial designation because it meets minimum parcel size, maximum lot coverage, building height, and all setback requirements.

The project also meets the design criteria outlined in Policy OSRC-5b because the architecture, and building bulk and mass are compatible with adjacent development. The site’s interior location off the main public street frontage (Aviation Boulevard) and behind existing office development minimizes the impact of the additional height requested and shields parking areas from public view. The orientation of the frontage and parking are appropriate because NeilMed owns adjacent properties, and the intention is to relate this development with the existing and future NeilMed sites. Existing oak trees will be preserved or relocated onsite. Site and building lighting are pedestrian oriented and no more intense than what is needed to serve the use.

The project meets the purpose of the Industrial Park land use designation in the Airport Industrial Area Specific Plan in that it is a light industrial activity that is compatible with adjoining land uses. Warehousing is an allowable use in the Specific Plan and the project meets the Industrial Park site development standards.

Design Considerations

Site Plan



At preliminary review, the Committee recommended that the site plan be reworked to accommodate the required parking space dimensions and availability, ADA path of travel width, and landscaping requirements. The applicant adjusted the site plan to meet all requirements.

The applicant also prepared a conceptual development plan showing the layout, scale, and massing of the adjacent vacant parcel under common ownership, as requested by the Committee.

Parking Design

The Sonoma County Zoning Code requires one parking space per 2000 square feet of warehouse space, and one space per 250 square feet. The project is a 59,066 square-foot warehouse with 500 square feet of the total reserved as office space, therefore 32 spaces are required. With the site plan adjustments, 43 parking spaces are planned onsite. Fourteen of those spaces are existing and shared with the property to the south, so 29 would be dedicated for the project. During preliminary review, the Committee provided general support for a parking reduction of up to 20 percent pursuant to Sec. 26-86-010(m) of the Zoning Ordinance due to the project's provision of bicycle parking, two electric vehicle charging stations, and reservation of three parking spaces for clean air or vanpool vehicles. A minimum of 26 spaces would be required after the reduction, which the project meets. Further, it is assumed that some of the 14 shared spaces will be available for onsite needs. Staff finds that adequate parking for the project is provided such that public health and safety will not be compromised and the incorporation of amenities encourage greater transportation sustainability.

Architecture

The Committee provided general support for the height limit exception to allow a 50-foot tall building, but recommended that the east elevation be enhanced to articulate the building entry. The applicant reduced the bulk of the projected roof element over the upper balcony to better define the central entry façade and also added windows at the ground floor level on the east and south elevations.

Environmental Analysis

An Initial Study and Mitigated Negative Declaration (ISMND) were prepared pursuant to the California Environmental Quality Act. The applicant has agreed to the mitigation measures to avoid or reduce to a less-than-significant level potentially significant adverse impacts on the environment, and the mitigations have been incorporated as conditions of approval. Potential environmental impacts have been identified in the following topic areas: Air Quality, Biological Resources, Cultural Resources, Greenhouse Gas Emissions, and Noise. The ISMND was circulated for public review and comment from February 24, 2021 to March 26, 2021. No comments were received.

NEIGHBORHOOD/PUBLIC COMMENTS

One member of the public has submitted a comment supporting the project. No other public comment has been received to date.

ATTACHMENTS

1. Proposal Statement
2. DRC Record of Action, December 18, 2019 and Applicant's Response
3. Staff Memo, Preliminary Design Review, December 18, 2019



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4. Final Project Plans (architectural, landscaping, electrical, and civil)
5. Colors and Materials, and Lighting Specifications
6. Mitigated Negative Declaration and [Attachments](#), February 24, 2021
7. Draft Conditions of Approval, April 7, 2021





**Proposal Statement
New Neilmed warehouse building
August 22, 2019**

Applicant/Agent: **Del Starrett Architect**
3663 North Laughlin Road suite 207
Santa Rosa, CA 95403

Owner: **Neilmed Pharmaceuticals Inc.**
601 Aviation Blvd.
Santa Rosa CA, 95403

Location: **685 Aviation Blvd, Santa Rosa,CA**

APN: 059-340-056

Site Size: 1.53 Acres

Zoning: Business park Industrial

Proposal: Proposed new 59,066 Sq. ft. two story warehouse building with 500 +- sq. ft. of it being office use. Two recessed loading dock areas and three surface level loading doors. The building footprint is 29,533 Sq. Ft.

1. PROJECT DESCRIPTION
1.1 SETTING

Location

The site is at the end of a private road adjacent to the Neilmed Offices and warehouse buildings.

The Site had a previously approved warehouse building and is adjacent to another industrial building on a lot to the south. It shares a parking and access easement with this property to the south.

Existing Uses

Open land with some parking and shared access aisle built with the property to the south.

Topography

Topographically the subject property is a generally flat site .

Vegetation

Site vegetation consists primarily of some small existing perimeter trees planted near the property lines. See the landscape plan.

Drainage

The paved portion of the property drains by sheet flow in a southwesterly direction. A grading and drainage plan has been provided by Brelje and Race.

Surrounding Land and Land Uses

The property site and surrounding lands are mostly existing Industrial uses.

To the North existing warehouse and industrial building.

To the East industrial park land and existing Neilmed building paved parking area.

To the West, Existing warehouse and industrial building.

To the South, Existing warehouse and industrial building.

1.2 PROJECT STATEMENT

The purpose for the warehouse is to supplement the company's existing warehouse buildings on the adjacent property.

Deliveries from UPS, FEDEX ,and US Mail are anticipated to the office area.

Semi Truck deliveries will be to the loading facilities on the north side of the building.

They would utilize existing roadways and the existing driveway to access the site from Aviation Blvd.

1.3 DETAILED PROJECT DESCRIPTION**Site Plan**

Building will be accessed from the existing driveway off Aviation Blvd that serves 601 Aviation. There is also an access easement from Aviation Blvd. over the adjacent lot to the south. 601 Aviation and 685 Aviation are owned by the same owner. These buildings are for Neilmed. A joint access agreement will be recorded so cars and trucks can access the new and existing buildings.

The office area and main entry is accessed on the east side of the building. This is where the office parking is provided. The three disabled access parking spaces are located in this area. Parking for six bikes is provided.

Truck access Semi truck deliveries will be to the recessed dock areas on the north side of the building. Two semi trucks per day are anticipated. One smaller box truck is anticipated per day that would use the surface level loading doors adjacent to the recessed dock areas.

Trucks will enter onto the site at the existing driveway access. They can fully pull onto the site and to the east area and back into the docks.

On departure they can pull ahead and turn south to the driveway. Brelje and Race has provided a truck movements study that shows that the proposed movements work properly. Exhibits for this are included with the submittal package

Parking requirements in the Airport Business park is one parking space for every 2,000 Sq. Ft. of Warehouse space. 59,066 Sq. ft. divided by 2000 = 29.53 spaces rounded up to 30. Spaces provided on the plan is 38.

Waste Disposal

Sanitary wastewater will be discharged into the existing sewer. There are no drains in the warehouse floor area.
No Industrial wastewater is anticipated

Water Supply

Water supply will be provided by an existing water main.

Hours of Operation:

The warehouse days and hours of operations are as follows:
Weekdays 5:00 am to 12:00 midnight.

Number of Employees:

Twenty employees are anticipated for this operation.

Architecture

The warehouse is proposed to be precast concrete panels for the exterior walls.

The building is 50 feet tall. There are two warehouse floor levels / stories. These are served by two stair wells, a freight Elevator and Passenger elevator.

The central ground floor element of the East elevation is the office entry. The office entry area has projected Facia shade area. Storefront Glass and entry door are at this shade area.

The building will be Tex-coted with the color scheme depicted in a color rendering.

The upper level East elevation has an observation balcony. There are various window openings at the upper level for daylighting purposes.

Windows frames are aluminum with dual glazed high energy efficiency glazing.

There is no rooftop heating equipment proposed. The roof will have skylights proposed for day-lighting the warehouse.

Future solar collectors are anticipated.

Lighting

All exterior lighting fixtures will have sharp cut offs to protect adjacent property from glare.

General site lighting at employee parking and site circulation is proposed to be on daily from 6:00 pm to 1:00 am and longer in some areas for security. A fixture and photometric plan have been provided.

Drainage and Grading

A preliminary grading and drainage plan is provided by Brelje and Race Civil Engineers.

Fencing

Fencing is existing chain link on the East and North Elevations.

Trash Enclosure

Trash is collected and placed in the new trash enclosure on the southwest side of the site for pick up by the local Waste pickup Company. This is a Split face concrete block wall with a metal roof structure and steel framed gate doors.

Signage

The primary signage consists of wall mounted address identification as required by the Fire Department. This is the only signage proposed.

Water Usage

Water will be used for purposes as follows:
Restrooms, fire protection and Irrigation.

Bicycle parking is provided at the front entry as show on the site plan and details.

Workers may store bikes inside as well. A double rack is provided near the entry.

Existing Trees

There are small existing trees along the north and west property line on the property site and are shown on the Landscape plan.

Landscaping

A landscape planting and irrigation plan for the project has been included for review.

DESIGN REVIEW COMMITTEE RECORD OF ACTION

December 18, 2019

ITEM NO: 2
Time: 1:45 pm or after
File No.: DRH19-0007 (PLP19-0045); JD190007

Subject: New Warehouse, Airport Business Center
Applicant: Ken DiLillo
Staff: Katrina Braehmer

Location: 685 Aviation Blvd, Santa Rosa
APN: 059-340-056 **Supervisorial District:** No. 4

Proposal: Preliminary Design Review for a new 59,066 square foot, two-story industrial warehouse, with 500+/- square feet to be used as office space, on a 1.53-acre parcel located in the Airport Business Center

Last Review: (No Prior meeting)
CEQA Status: MND / Prior MND

Zoning: MP (Industrial Park), 1 Acre Average, VOH (Valley Oak Habitat)

ATTENDANCE

Committee: Don McNair, Jim Henderson, Derik Michaelson
Staff: Katrina Braehmer
Applicant: Ken DiLillo
Others: None

REVIEW LEVEL: Preliminary Final Review Concept / Referral

DRC ACTION: *

	Approval	Revisions	Final Details	Staff Review
<u>Project Design:</u>	X			
Site Plan:		X		
Architecture:		X		
Parking Design:		X		
Landscaping:				
Color/Materials:				
Signage:				
Lighting:				
Other:				

VOTE: Don McNair Jim Henderson Derik Michaelson

Ayes: 3 Noes: 0 Absent: 0 Abstain: 0

* see Comments & Conditions (attached)

**DESIGN REVIEW RECORD OF ACTION
COMMENTS & CONDITIONS**

Applicant: Ken DiLillo
Address: 685 Aviation Blvd, Santa Rosa
APN: 059-340-056

Date: December 18, 2019
File: DRH19-0007 (PLP19-0045)
Action: FINAL REVIEW COMPLETE

NOTE: Applicants shall submit project revisions for further consideration as specified below. A written response addressing each comment is required. Responses to final review comments shall be confirmed by planning staff before permit issuance.

GENERAL

1. See comments below

SITE PLAN

2. Rework overall site plan, parking design, and building footprint to accommodate, required parking space/curb depth dimensions, ADA path of travel width, Specific Plan landscaping requirements at 20% of lot, and unavailability of existing reserved spaces within neighboring parking easement
3. Provide conceptual development plan showing general layout, scale and massing for development of adjacent vacant parcel and last remaining lot under Parcel Map PLP98-0016.

ARCHITECTURE

4. DRC offers support for proposed height limit exception based on interior location of building site behind existing office development with minimal visibility from main business park frontage along Aviation Blvd.
5. Consider revising main façade elevation to enhance articulation of building entry.

PARKING DESIGN

6. DRC offers general support for requested parking reduction not to exceed 20 percent of required spaces, subject to responding accordingly to Site Plan comments 2 and 3 above,

LANDSCAPING

7. No significant concerns

COLORS / MATERIALS

8. No significant concerns

LIGHTING

9. No significant concerns

SIGNAGE

10. No significant concerns

OTHER

11. No significant concerns
-

PUBLIC COMMENTS

N/A Attached

ATTACHMENTS

N/A Public Comments
 Final Conditions Correspondence
 NOD / NOE Other: _____



March 25 2020
Katrina Braehmer
Planner II
County of Sonoma
2550 Ventura Avenue, Santa Rosa, CA 95403

Re: 685 Aviation Blvd.
Santa Rosa, CA 95403

Re: DRH19-0007 DRC Record of Action

Hi Katrina,
I hope you are doing well. I am working from home and I suppose you may be as well. We have revised the design drawings and documents with responses below in blue. I will send them electronically via email and can provide printed plans as well when the Permit Center reopens.

Thank you
Del

1. See comments below
SITE PLAN

2. Rework overall site plan, parking design, and building footprint to accommodate, required parking space/curb depth dimensions, ADA path of travel width, Specific Plan landscaping requirements at 20% of lot, and unavailability of existing reserved spaces within neighboring parking easement.

We have revised the site plan to achieve the minimum required parking of 30 cars. The parking easement of the south site is a shared parking easement. It covers 14 spaces. We assume that our site could share up to 7 of these spaces. We would only need to share one to have the 30 parking spaces required on site.

The parking space depth dimensions and curb overhang into landscape dimension are shown on the site plan. The ADA path of travel widths are indicated. Landscaping is at 20% of the lot.

3. Provide conceptual development plan showing general layout, scale and massing for development of adjacent vacant parcel and last remaining lot under Parcel Map PLP98-0016.

A conceptual development plan showing general layout, scale and massing for development of the adjacent vacant parcel is provided in a site plan, floor plan and conceptual elevations.

ARCHITECTURE

4. DRC offers support for proposed height limit exception based on interior location of building site behind existing office development with minimal visibility from main business park frontage along Aviation Blvd. [Understood](#)

5. Consider revising main façade elevation to enhance articulation of building entry. [The East elevation with the main building entry has been revised.](#)
[We redesigned the projected roof element over the upper balcony to better define the central entry elevation. We made it thinner and less bulky. We added a projected horizontal roof plane element above the soffited area. This is set at 2.5 feet above the main parapet height that is 50 Ft., to create some variation in height and make the central elevation stand out.](#)
[Per Design review comments we added more windows at the ground floor on the east and south elevations.](#)

PARKING DESIGN

6. DRC offers general support for requested parking reduction not to exceed 20 percent of required spaces, subject to responding accordingly to Site Plan comments 2 and 3 above, [LANDSCAPING Understood see responses to 2 and 3 above.](#)

7. No significant concerns
[COLORS / MATERIALS Understood](#)

8. No significant concerns
[LIGHTING Understood](#)

9. No significant concerns
[SIGNAGE Understood](#)

10. No significant concerns
[OTHER](#)

11. No significant concerns [Understood](#)

DESIGN REVIEW COMMITTEE MEMORANDUM

DATE: December 18, 2019
ITEM: No. 2 - 1:45 pm
FROM: Katrina Braehmer, Project Planner

SUBJECT: File No.: **DRH19-0007**; NeilMed Warehouse
Applicant: Ken DiLillo, on behalf of NeilMed Pharmaceuticals Inc.
Address: 685 Aviation Blvd., Santa Rosa
APN(s): 059-340-056

REQUEST

Ken DiLillo, on behalf of NeilMed Pharmaceuticals Inc., requests preliminary design review for construction of a new two-story, 59,066-square foot warehouse building with two loading docks on a 1.53-acre parcel. Other site improvements include paved parking and circulation areas, landscaping, and site lighting.

BACKGROUND

The 1.53-acre parcel is zoned MP (Industrial Park), which allows industrial warehouse uses by right. The site is located down a private road off of Aviation Boulevard, within the Airport Business Center and the purview of the Airport Industrial Area Specific Plan. The site is mostly undeveloped, with the exception of a paved parking area along the southern edge of parcel, which is utilized by the neighboring parcel via a parking and access easement. The site and the parcel to the south were originally a single lot, prior to its subdivision in 1998. The minor subdivision was approved jointly with a use permit for planned development status to allow for future construction of two industrial buildings, one on each lot, that did not meet the 40' average setback required by the Airport Industrial Area Specific Plan. Separate design review applications were approved for both buildings, though only the southern parcel was developed. The Specific Plan is in the process of being updated, and no zoning or land use changes are proposed for the project site or abutting parcels.

NeilMed Pharmaceuticals Inc. also owns the parcel to the northeast (APN 059-271-044), which is currently utilized for office and warehouse space, and the parcel directly east (APN 059-430-007), which is currently undeveloped. The proposed building will be accessed from the private road that is part of the northeastern NeilMed parcel.

PROJECT DESIGN HIGHLIGHTS

- Two-story 59,066-square foot building with a footprint of 29,533 square feet (44% lot coverage)
- Flat roof, 50' in height
- Two loading docks accessed from the front of the parcel
- Front façade features a central entrance with a canopy, a second floor balcony, and spandrel glass
- Exterior walls are composed of precast concrete panels in an earth-tone color scheme



DISCUSSION

1. Parking

Minimum Parking

The proposed site plan depicts 38 parking stalls on the subject parcel. The minimum required for the new use is 32. The project parcel and the parcel to the south were originally a single lot, prior to its subdivision in 1998. On the parcel map, temporary and permanent parking easements were placed on the project parcel in favor of the neighboring parcel. The permanent easements were required to meet the provision of parking necessary to approve the use on the neighboring parcel to the south. The permanent easements therefore encumber 15 of the existing parking stalls on the project site, which effectively removes these spaces from counting toward the required number for the proposed use. For that reason, the project is nine spaces short of the required 32.

Parking regulations in the Zoning Code allow for reductions in the required parking as follows:

- *Sec. 26-86-010(i): The above parking standards may be reduced when it has been satisfactorily demonstrated to the planning director or applicable decision-making body that fewer spaces will adequately serve the specific use or that the applicant has encouraged transit opportunities through one or more of the following:*
 - *Participation in a comprehensive travel demand management (TDM) program including, but not limited to, provision of flex-time, carpooling, and transit passes such that VMT generated by the project is reduced;*
 - *Provision of transit stops and/or turnouts developed in cooperation with and approved by Sonoma County Transit;*
 - *Provision of amenities for bicyclists, bus riders, carpoolers and pedestrians beyond those required by this section.*
- *Sec. 26-86-010(m): The parking required may be reduced by up to twenty percent (20%) of total required spaces when it has been satisfactorily demonstrated to the planning director or applicable decision-making body that:*
 - *Adequate parking for the projected use is provided such that the public health and safety is not compromised; and*
 - *That such design provides greater sustainability through the incorporation of amenities to encourage alternative modes of transportation and VMT reduction to offset GHG emissions from vehicle trips; and/or that*
 - *The reductions are required in order to implement Low Impact Development (LID) parking lot design, including onsite retention and treatment of stormwater.*
 - *Examples include the provision of electric vehicle charging stations, covered secured bike parking or bike lockers, showers and changing areas for employees, vegetated swales, and connections to Class I facilities. The parking required may be reduced by ten (10) percent when at least ten percent (10%) of the total spaces are provided as covered by solar panels. These reductions may be combined but the total reduction in the required number of parking spaces may not exceed twenty percent (20%).*



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Four of the proposed parking stalls would have electric vehicle charging stations and two stalls would be reserved for clean air vehicles or vanpool. Using Sec. 26-86-010(m), the required parking can be reduced by up to six spaces, for a total of 26. Even if the maximum reduction is permitted through the proposed amenities to promote sustainable modes of transportation, the project would still have a shortage of three spaces. Limiting the intensity of the use – thus the required parking – by reducing building floor area might be necessary to meet the minimum parking requirements.

Stall Length

Sec. 26-82-030(l) of the Zoning Code requires dimensions of 9 by 20 feet for standard parking stalls, and 8 by 16 feet for compact stalls. The proposed striping for standard stalls does not meet the minimum dimensions at 17 feet in length. At the planned building footprint, and with other constraints including landscaping requirements, minimum aisle widths, and setbacks, there is no space to extend the stall lengths. The Committee could consider the landscaped strips abutting most of the parking areas as extensions to the striped stall length to accommodate additional vehicle length. However, this option is not recommended for the six stalls abutting the sidewalk to the main entrance of the building as it would impact accessibility. Another option to meet the required stall length is to reduce the building's footprint to allow for the full striped length, which might nevertheless be unavoidable to accommodate the required number of stalls as discussed above.

Access and Parking Orientation

The applicant proposes access from the private road on the easterly side, with pull-in parking for passenger vehicles sited directly along the roadway. Passenger vehicle access to the proposed building would also be available through an existing access easement along the parking lot of the parcel south of the project site. General development standards in Sec. 26-82-030(q) of the Zoning Code instruct that, "except for parking areas accommodating three (3) or fewer vehicles, a car entering a street or highway can do so by traveling in a forward direction". With the proposed project, the private road would serve at least two parcels, not considering future development of the parcel directly east of the project site. After consultation with the Department of Transportation and Public Works, staff finds that the pull-in parking design is low-risk given that the road has minimal traffic and low speeds, and as long as vegetation does not obscure sight lines.

2. Landscaping

On parcels designated as Industrial Park, the Airport Industrial Area Specific Plan requires a minimum of 20 percent of the site to be landscaped. The current site plan illustrates 20.3 percent of the site is reserved as landscaping. Two additional landscaping standards, which are not met by the proposed site plan, are as follows:

- *"All development sites within an MP district shall reserve a minimum 25-foot wide area (from the adjoining street curb line) to be used exclusively for landscaping. The owner, lessee, or occupant shall be responsible for landscaping and maintaining this area."*
- *"A minimum of 50 percent of the area between the curb line and the minimum building setback line shall be mounded and bermed and planted in lawn, or in a suitable substitute established in a landscape master plan approved by the Design Review Committee."*



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The parcel does not adjoin a public street, and instead is accessed by a private road that is part of a separate parcel. For development review purposes, the easterly property line is considered the front of the parcel because of the proposed orientation of the building and access to the site. If understanding the private road as the adjoining street, the proposed plan does not reserve a 25-foot wide area from the “curb line” for landscaping, and does not comply with the required mounding and berming. Instead, the area along the roadway is mostly proposed as parking and a landscaped buffer will border the front edge of the building. Mounding and berming are not present on the parcel to the south of the project site, but are utilized along the frontage of the other two parcels east of Skylane Boulevard on Aviation Boulevard.

Inverting the landscaping and parking orientation described by the Specific Plan and implemented by the parcel south of the project site impacts the cohesion between developments along the private road. The applicant has indicated that the intent for the site’s layout is to relate this parcel with the neighboring two NeilMed-owned parcels to create a campus-like design with connecting circulation. Staff recommends the Committee consider how the project’s proposed landscaping and parking orientation influence the continuity between current and future developments along the private road.

3. Building Height

The Specific Plan applies a 28-foot height limit between the 25-foot minimum and 40-foot average setback lines. The height criteria reads:

“Structures shall not exceed 28 feet in height at any building setback line. Between the 25-foot minimum and 40-foot setback lines, structures shall not exceed 28 feet in height. For each foot of setback interior to the 40 foot building setback line, an additional 6 inches of building height shall be permitted; the total height shall not exceed 50 feet. Additional height may be permitted under stringent special use permit procedures only. Heating, cooling, and other roof equipment should be included in these building height restrictions.”

In addition to the Design Review request, the applicant requests a Use Permit for a 50-foot building height. The proposal does not include rooftop equipment that would increase the total height above the 50-foot maximum. In comparison, the approved building height for the NeilMed building to the northeast is 25 feet and approximately 26 feet for the building on the lot south of the project site.

RECOMMENDATION

Staff recommends the Committee provide preliminary feedback regarding the following:

- Site design, including access and circulation, parking requirements and stall dimensions, and landscaping
- Building design, including a request for additional height, and materials and colors
- Exterior lighting



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Attachments

1. Applicant project statement
2. Project plan set, including landscaping and preliminary drainage plans
3. Assessor's parcel map
4. Parcel Map PLP98-0016
5. Materials and colors board
6. Lighting specifications
7. Photos of the site and surrounding area
8. Code References



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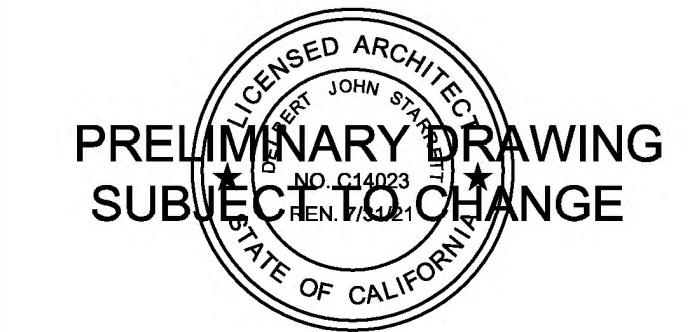


INDEX OF DRAWINGS

- A0.0 PROJECT INFORMATION
- A1.0 SITE PLAN
- A1.1 VICINITY PLAN
- A1.2 VICINITY AERIAL PLAN
- A2.0 FLOOR PLANS
- A3.0 ELEVATIONS
- A4.0 BUILDING SECTIONS
- A5.0 SITE FEATURES
- A6.0 NEILMED 2 CONCEPTUAL SITE PLAN
- A5.0 NEILMED 2 CONCEPTUAL ELEVATIONS
- C1 EXISTING CONDITIONS
- C2 PRELIMINARY SITE PLAN
- C3 PRELIMINARY GRADING PLAN
- C4 PRELIMINARY UTILITY PLAN
- C5 PROPOSED SWLID EXHIBIT
- C6 DETAILS
- C7 DETAILS
- T1 FIRE TRUCK TURNING TEMPLATE
- T2 WB-65 TRUCK TURNING TEMPLATE
- L1 LANDSCAPE PLANTING PLAN
- L2 LANDSCAPE IRRIGATION PLAN
- E1 SITE ELECTRICAL WITH PHOTOMETRICS

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685 AVIATION BLVD
SANTA ROSA CA 95403

PROJECT DATA

PROJECT TEAM

CIVIL: BRELJE AND RACE
475 AVIATION BLDG SUITE 120
SANTA ROSA, CA 95403
707-576-1322

LANDSCAPE: PARKER SMITH
1945 PINER #25
SANTA ROSA, CA 95403
707-477-7502

ELECTRICAL: LUNARDI ELECTRIC
5334 SEBASTOPOL ROAD
SANTA ROSA, CA 95407
707-545-4755

ASSESSOR PARCEL: 059-340-056

SITE AREA : 66,437 SF 1.53 AC.±

BLDG FOOTPRINT AREA :
1ST FLOOR: 29,533 SF
2ND FLOOR: 29,533 SF
TOTAL: 59,066 SF

BUILDING	29,533 SF	44.5%
LANDSCAPE	13,353 SF	20.1%
AC PAVING	23,551 SF	35.4%

PARKING TABULATION
59,066 SF WAREHOUSE
@ 1 PER 2000 SF = 30 CARS REQUIRED

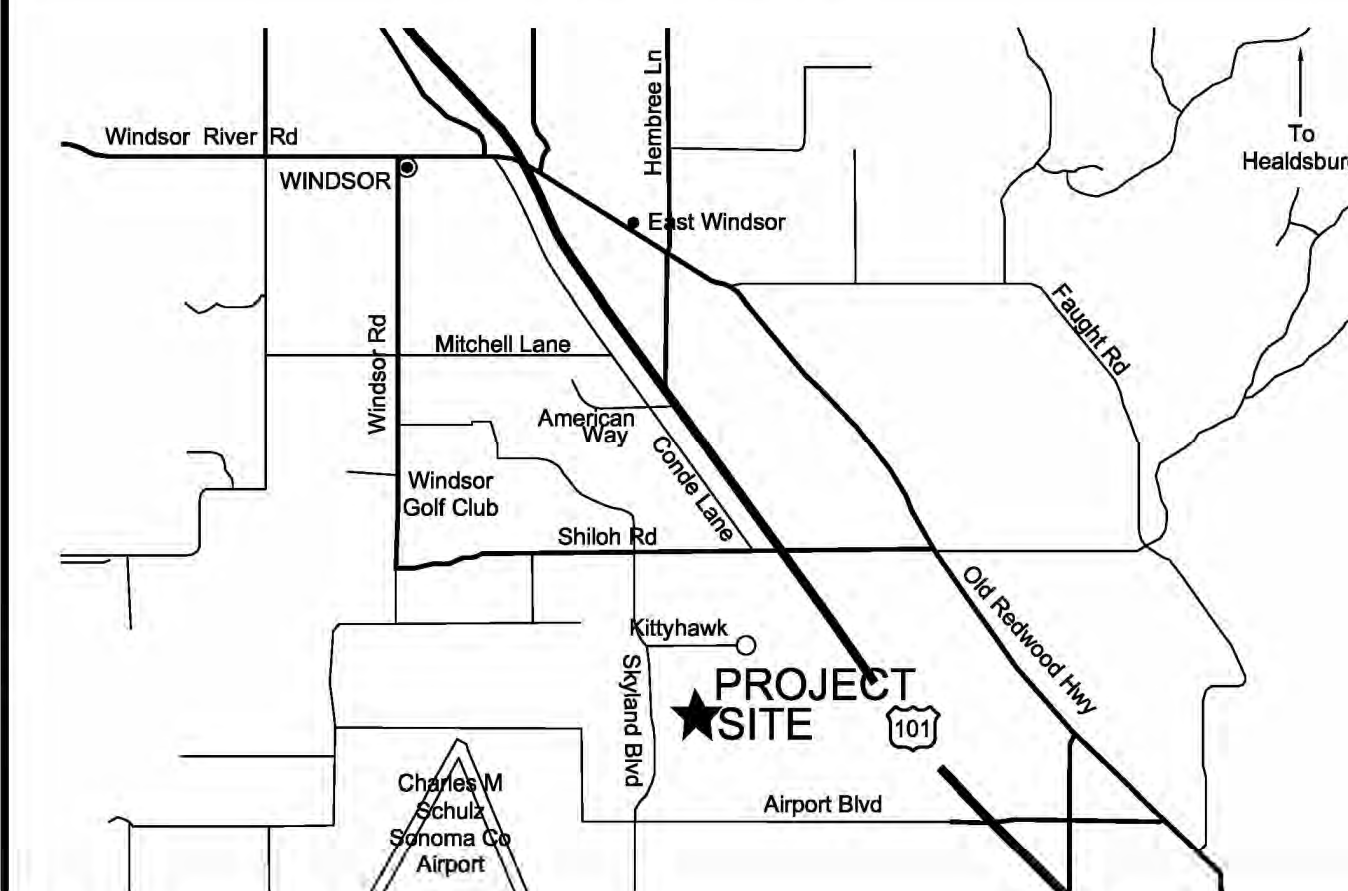
TOTAL PARKING PROVIDED = 36 CARS
29 + 7 WITHIN PARKING EASEMENT
(7 OF THE 14 ALLOCATED TO ADJACENT PROPERTY)

2 ACCESSIBLE PARKING REQUIRED
6 CLEAN AIR, VANPOOL EV REQUIRED

AERIAL SITE



VICINITY MAP



3/15/20	DESIGN REVIEW SUBMITTAL 2
8/25/19	DESIGN REVIEW SUBMITTAL 1

REV	DATE	DESCRIPTION
		PROJECT NO: 1807
		CAD DWG FILE: A00
		DRAWN BY: KCT
		SCALE: NONE

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SHEET TITLE

PROJECT INFORMATION

SHEET NUMBER
A0.0



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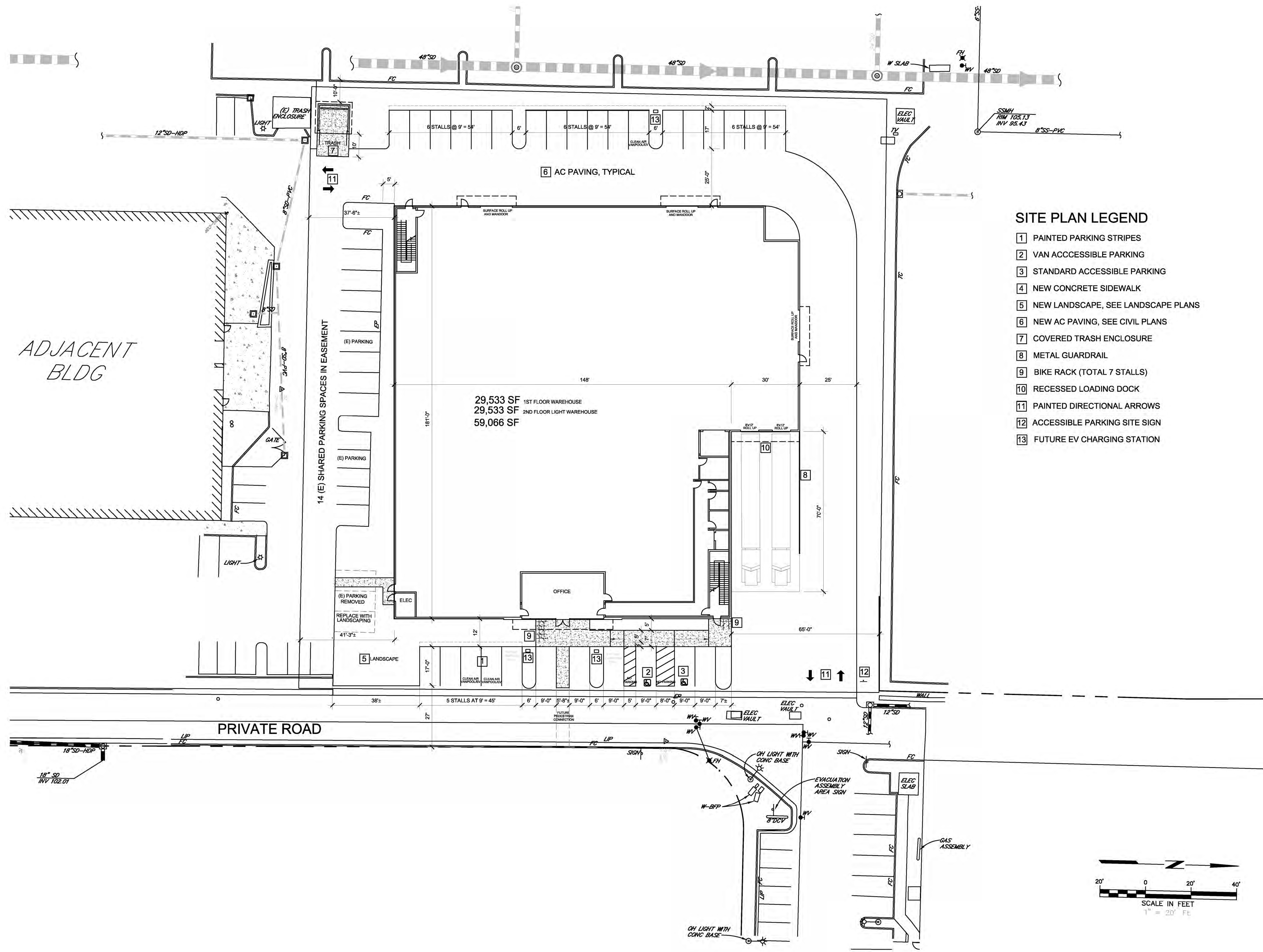
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SHEET TITLE

SITE PLAN

SHEET NUMBER

A1.0



SITE PLAN LEGEND

- 1 PAINTED PARKING STRIPES
- 2 VAN ACCESSIBLE PARKING
- 3 STANDARD ACCESSIBLE PARKING
- 4 NEW CONCRETE SIDEWALK
- 5 NEW LANDSCAPE, SEE LANDSCAPE PLANS
- 6 NEW AC PAVING, SEE CIVIL PLANS
- 7 COVERED TRASH ENCLOSURE
- 8 METAL GUARDRAIL
- 9 BIKE RACK (TOTAL 7 STALLS)
- 10 RECESSED LOADING DOCK
- 11 PAINTED DIRECTIONAL ARROWS
- 12 ACCESSIBLE PARKING SITE SIGN
- 13 FUTURE EV CHARGING STATION



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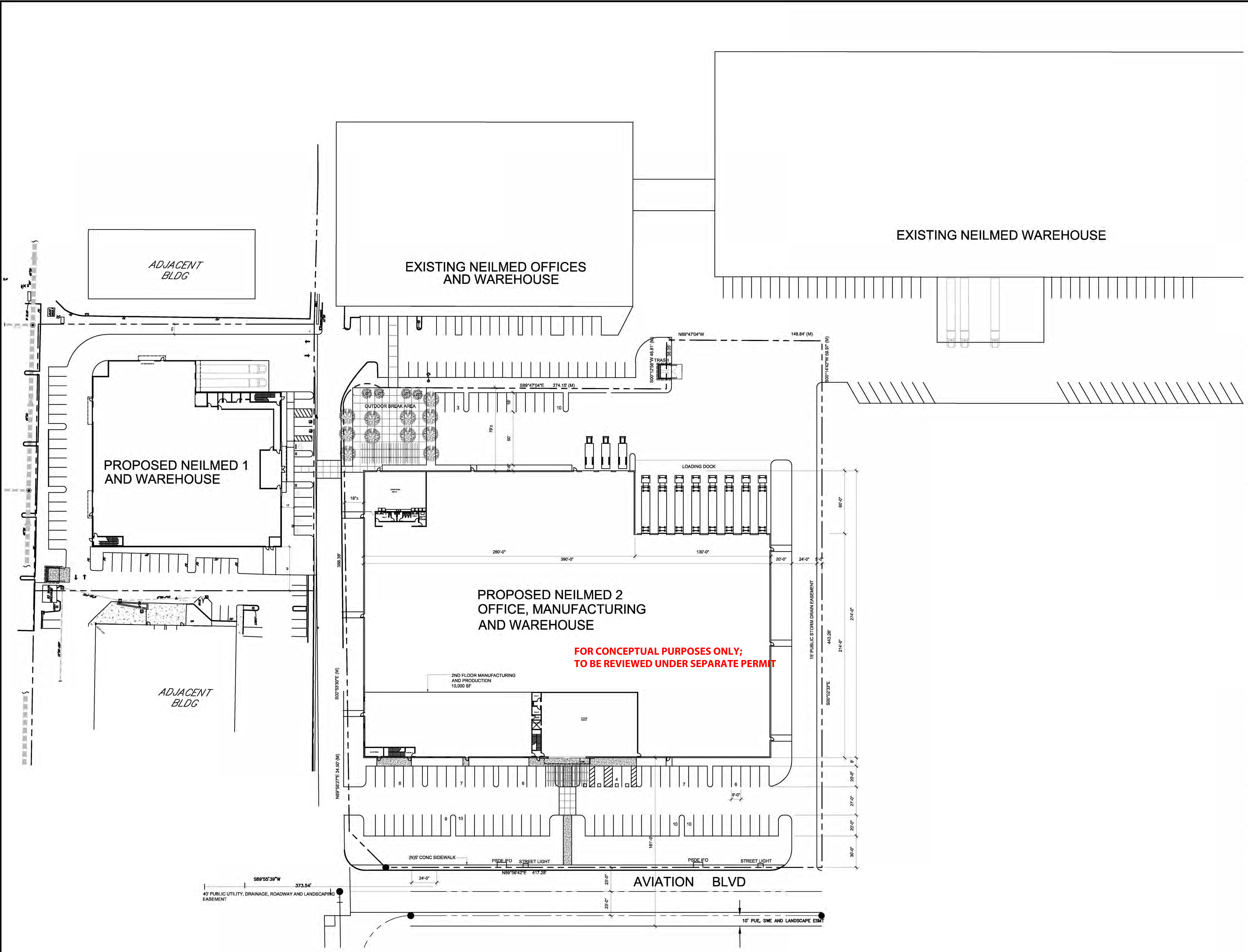
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SHEET TITLE

VICINITY PLAN

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A1.1

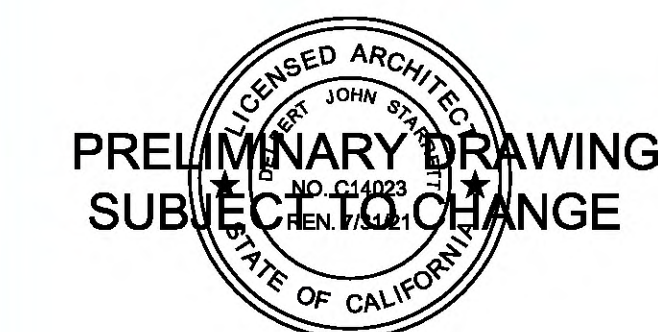




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PRELIMINARY DRAWING
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PROJECT NO:	1807
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SHEET TITLE

VICINITY
 AERIAL PLAN

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A1.2

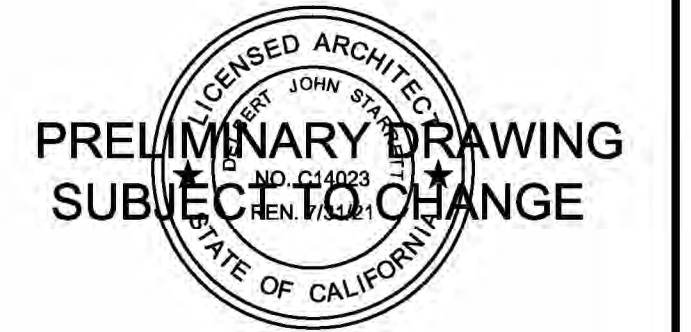




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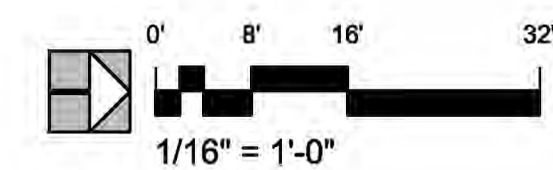
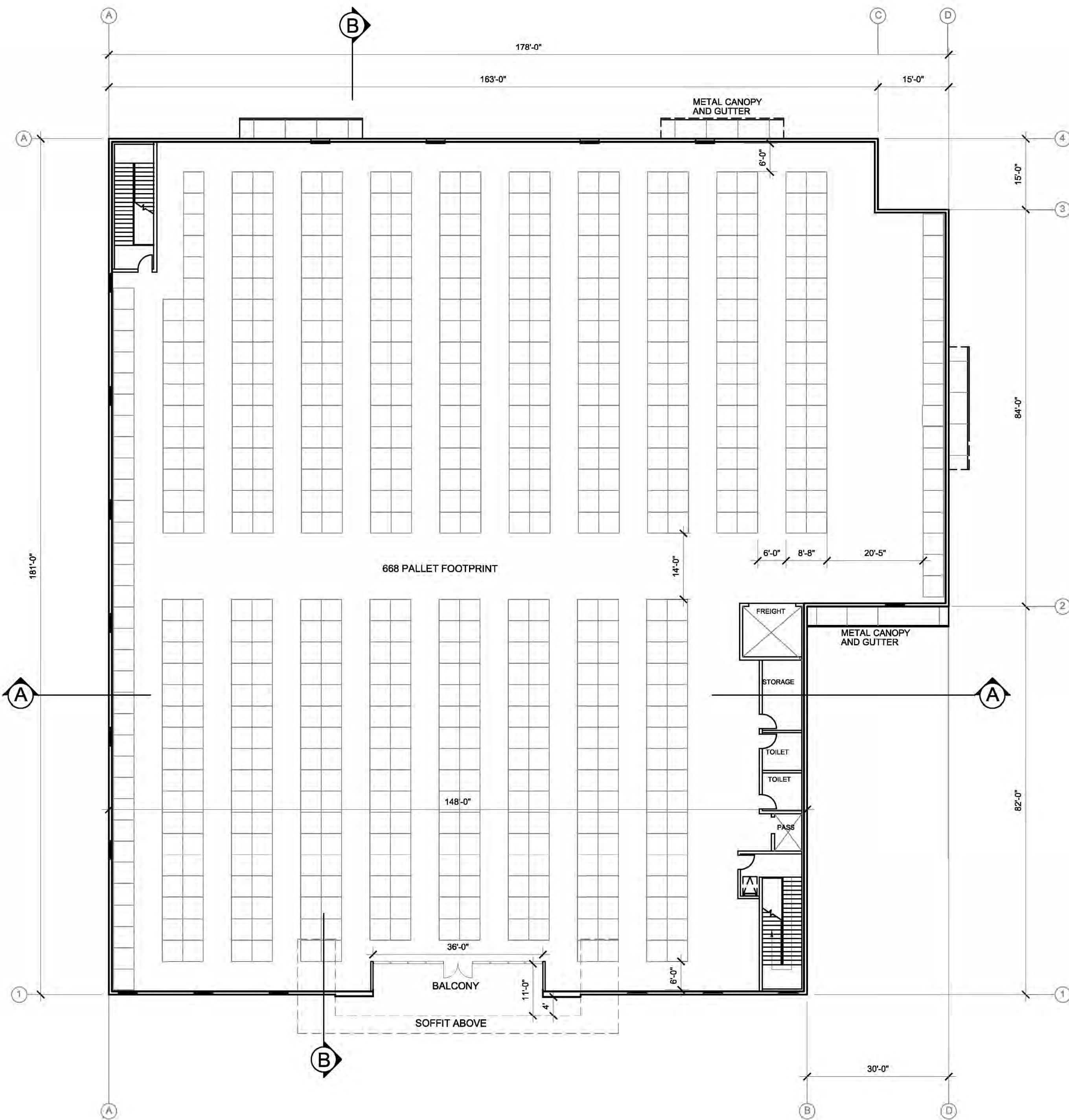
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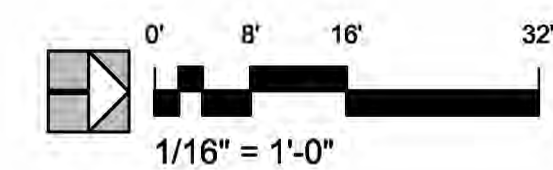
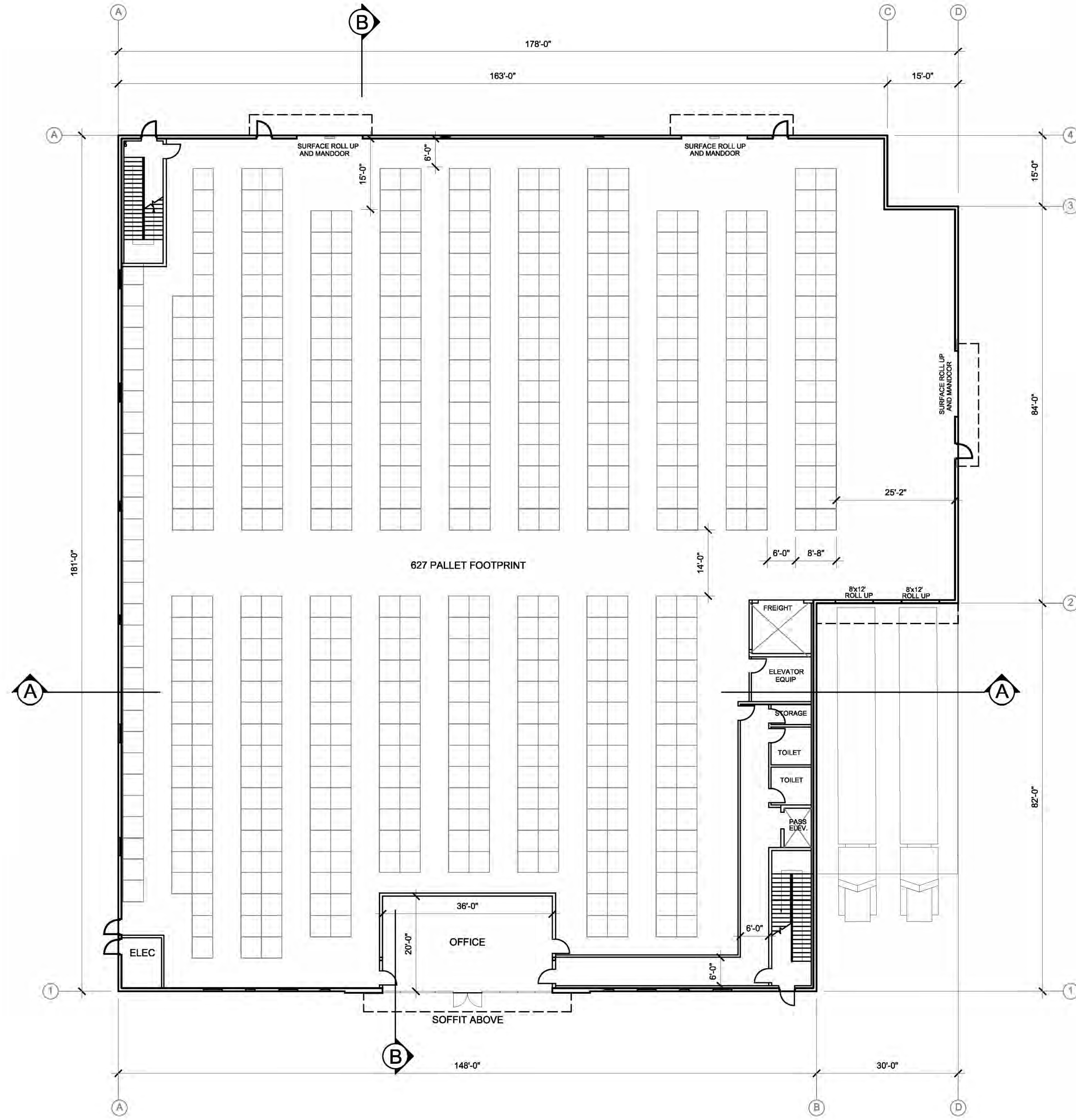
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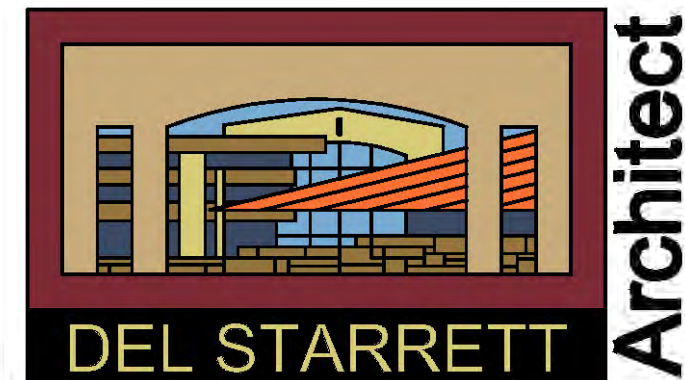
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2ND FLOOR PLAN



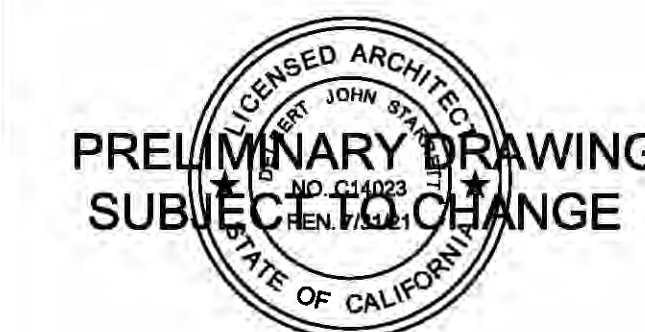
1ST FLOOR PLAN



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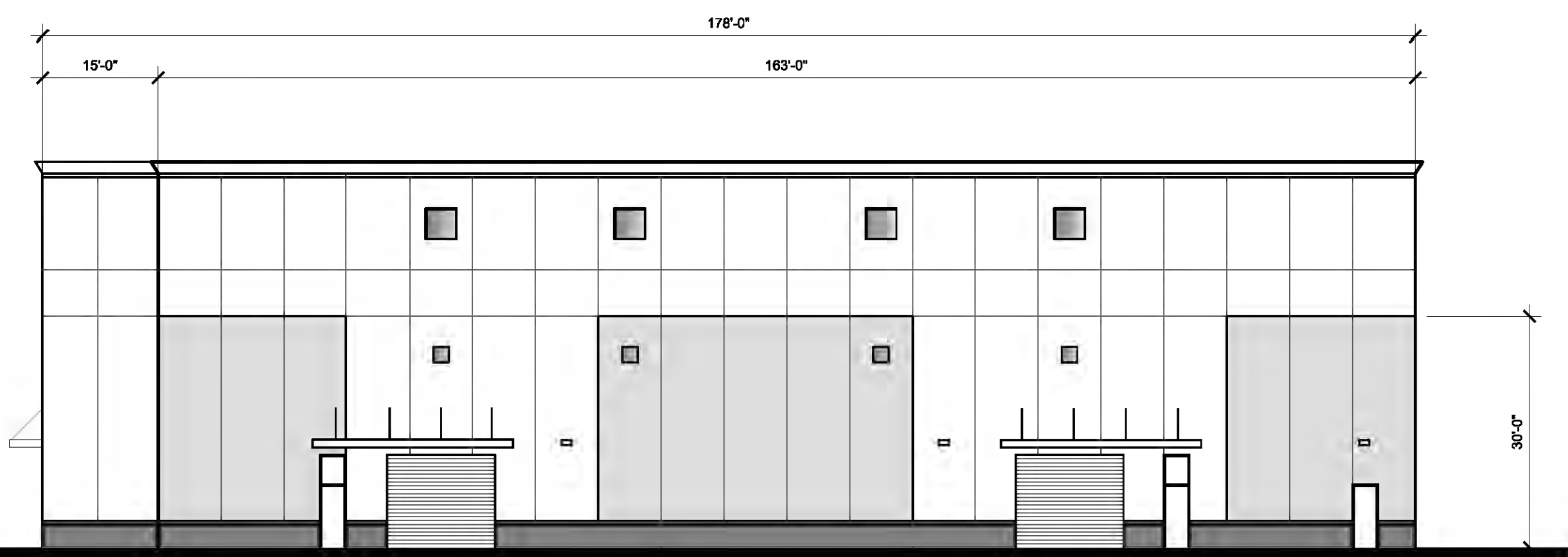
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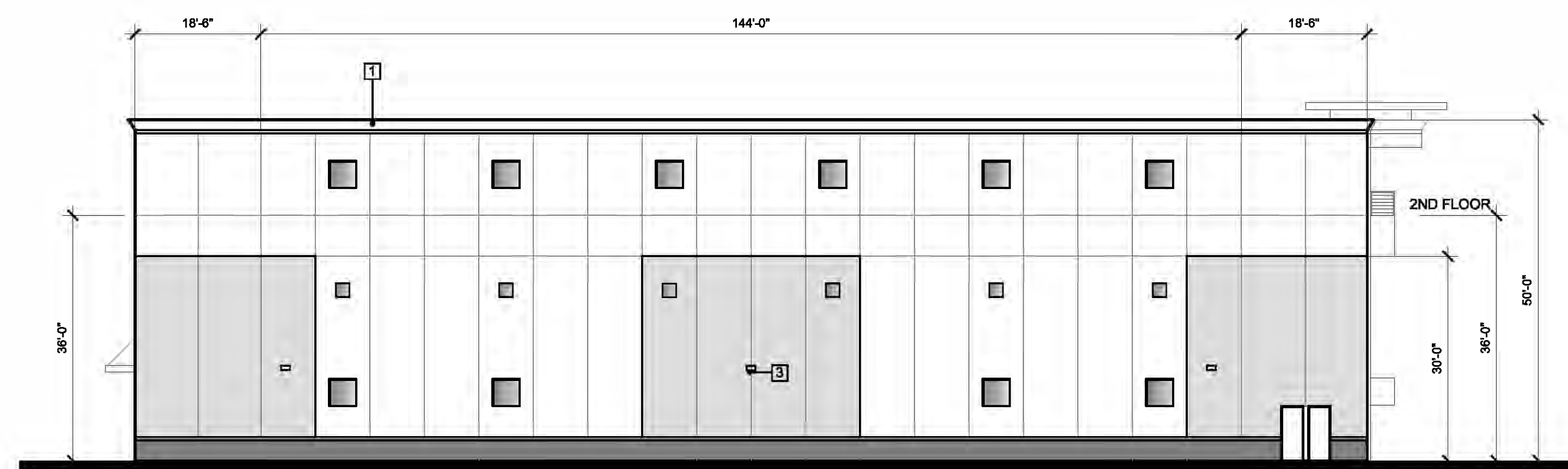
ELEVATIONS

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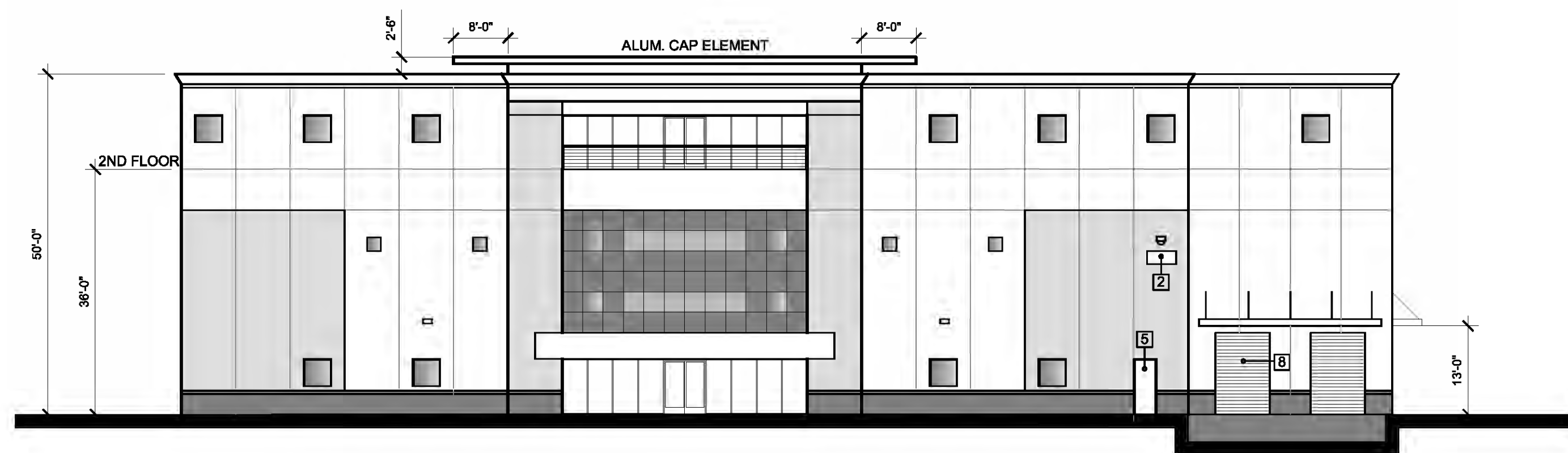
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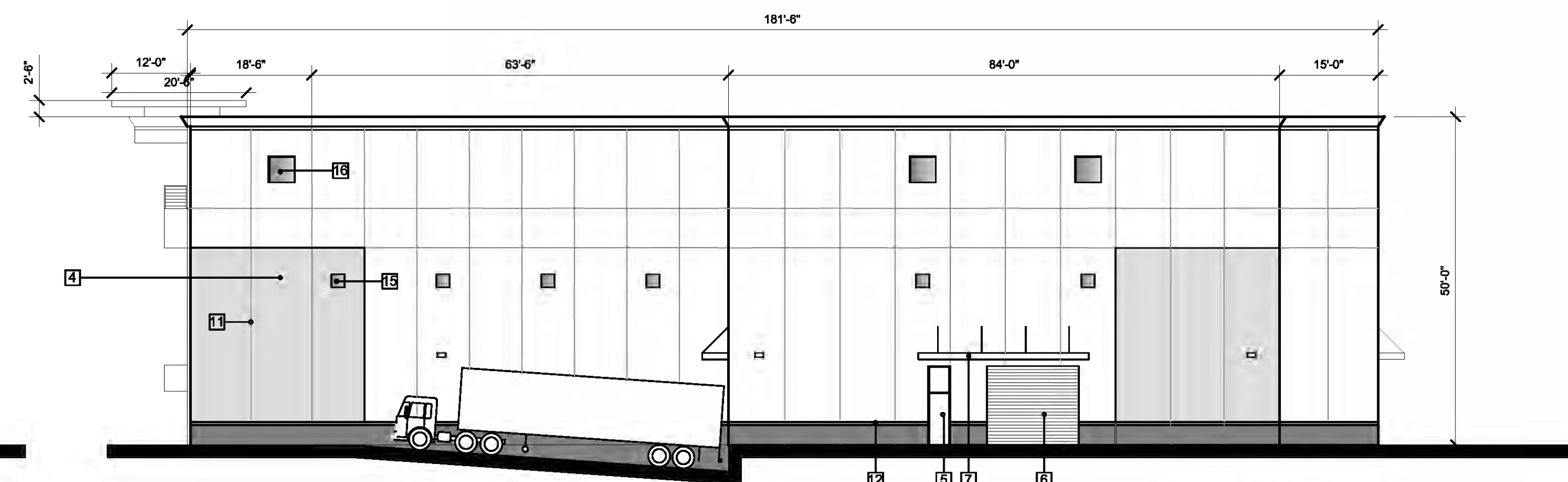
WEST ELEVATION



SOUTH ELEVATION



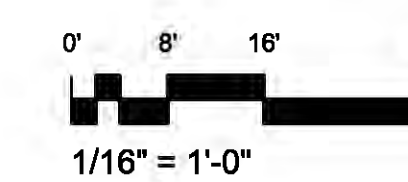
EAST ELEVATION



NORTH ELEVATION

LEGEND

- 1 CORNICE
- 2 12" ADDRESS SIGN EXTERNALLY ILLUMINATED
- 3 WALL MOUNTED LIGHT - SEE ELECTRICAL PLANS
- 4 12" THICK PRECAST CONCRETE PANEL
- 5 METAL MANDOOK
- 6 METAL SURFACE OVERHEAD DOOR
- 7 METAL CANOPY
- 8 METAL OVERHEAD DOORS AT RECESSED LOADING
- 9 ENTRANCE STOREFRONT
- 10 ALUM STOREFRONT WINDOWS
- 11 CONCRETE PANEL VERTICAL JOINT
- 12 HORIZONTAL REVEALS IN CONCRETE PANEL
- 13 2ND FLOOR BALCONY
- 14 42" GUARDRAIL
- 15 24" x 24" WINDOWS
- 16 48" x 48" WINDOWS
- 17 SPANDRAL GLASS
- 18 18" THICK PRECAST CONCRETE PANEL
- 19 10' DEEP ENTRY CANOPY

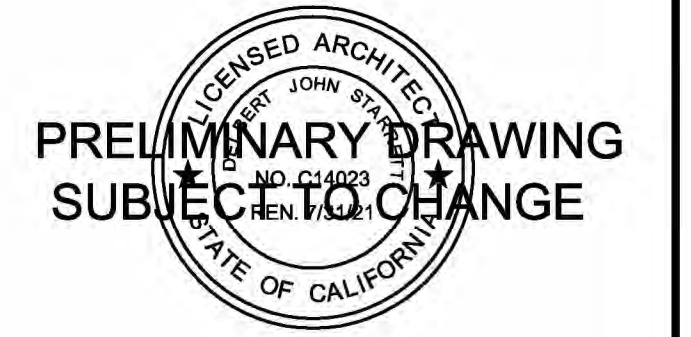




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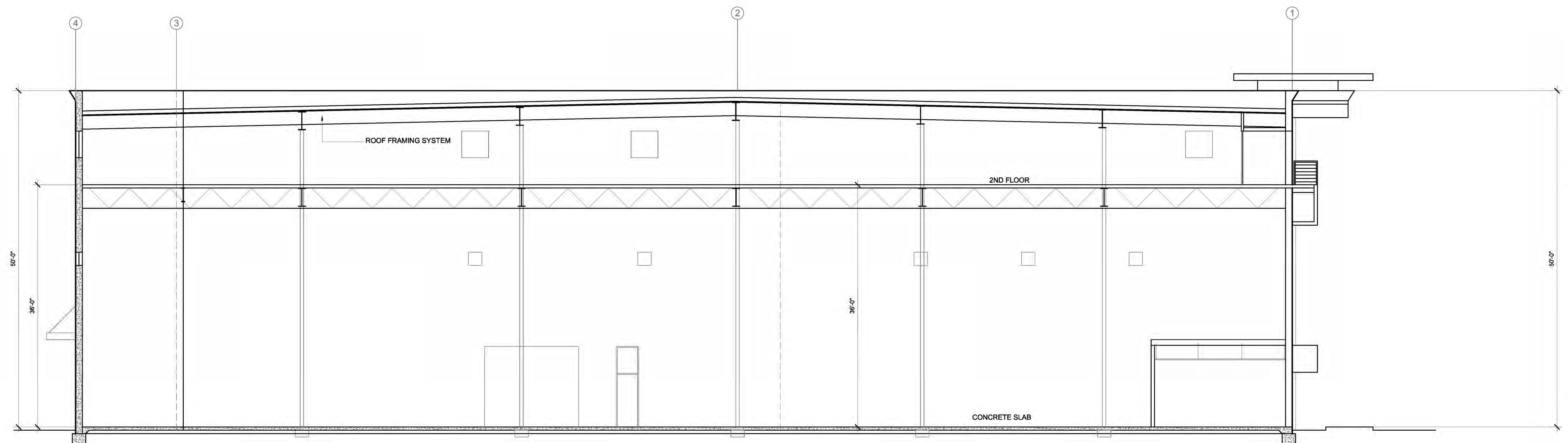
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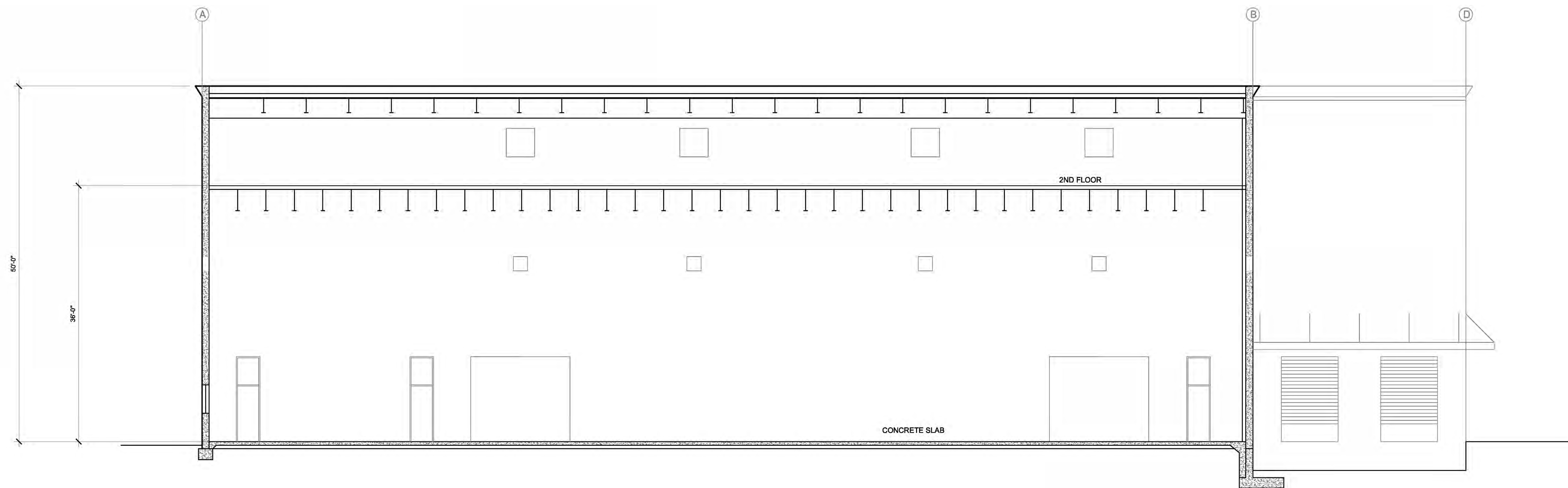
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SHEET NUMBER

A4.0



SECTION A-A



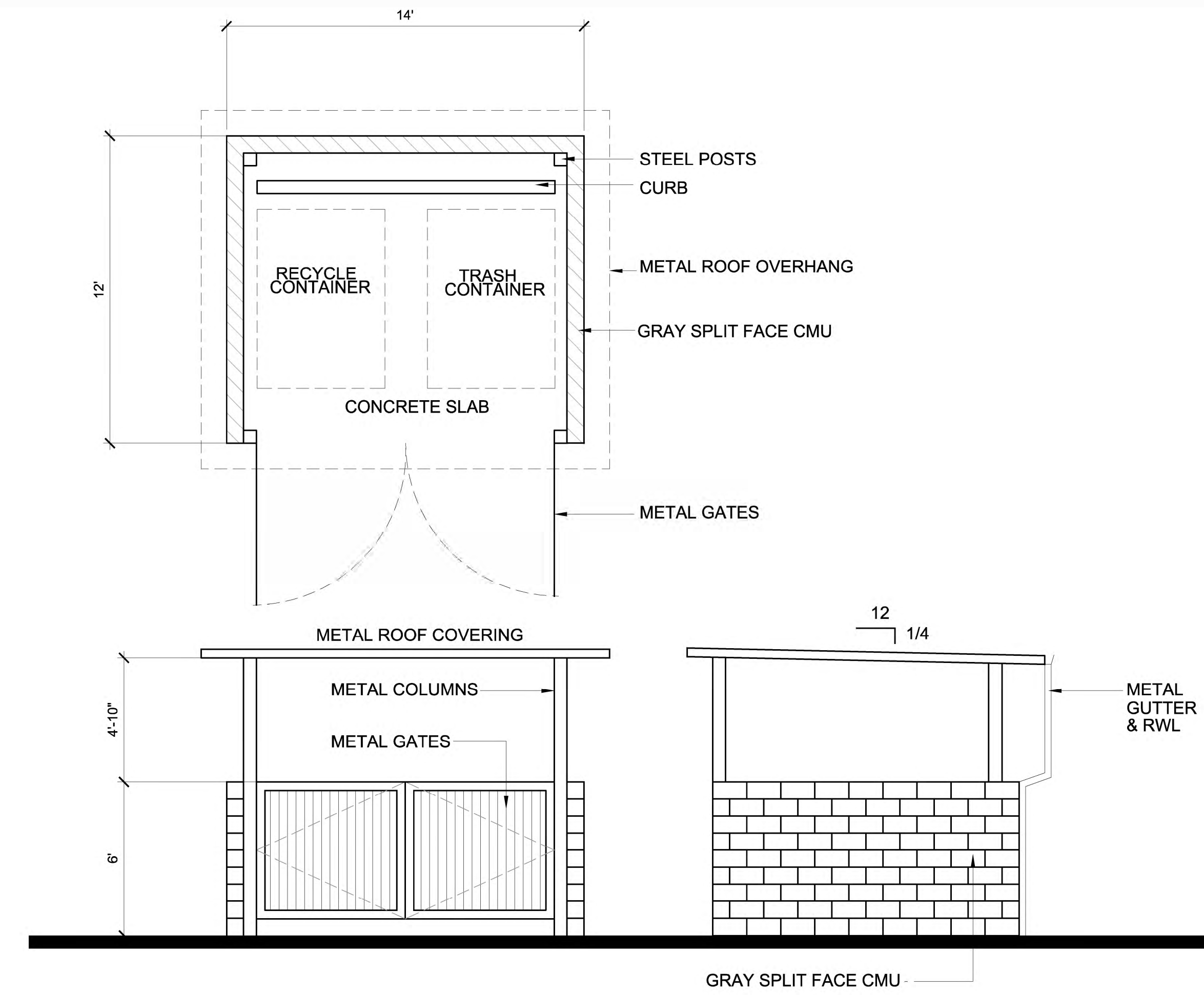
SECTION B-B



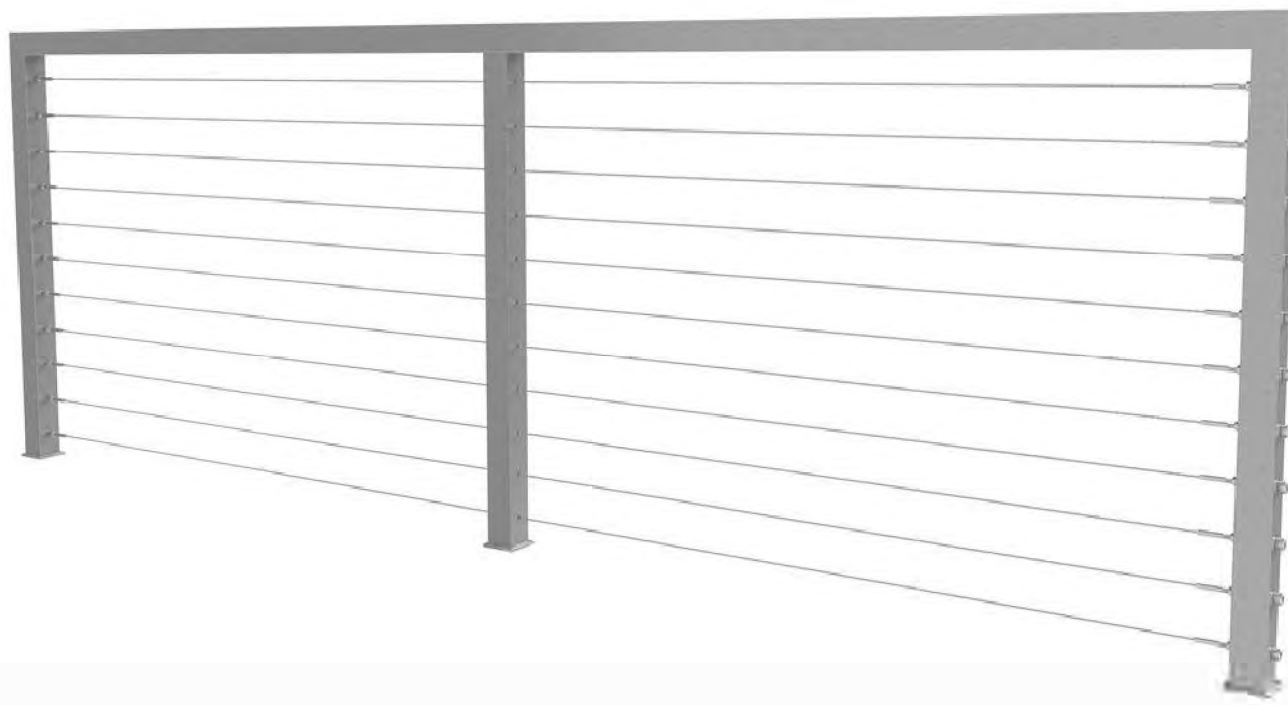
BALCONY GUARDRAIL



SITE POLE LIGHT FIXTURE



TRASH ENCLOSURE



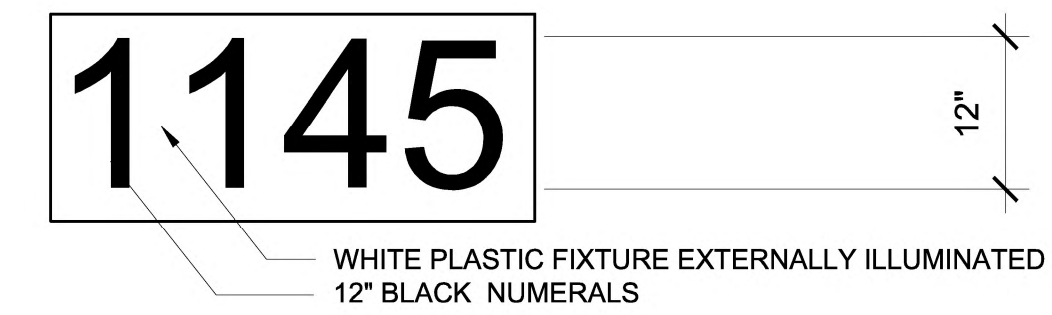
BALCONY GUARDRAIL DETAIL



BUILDING MOUNTED LIGHT FIXTURE



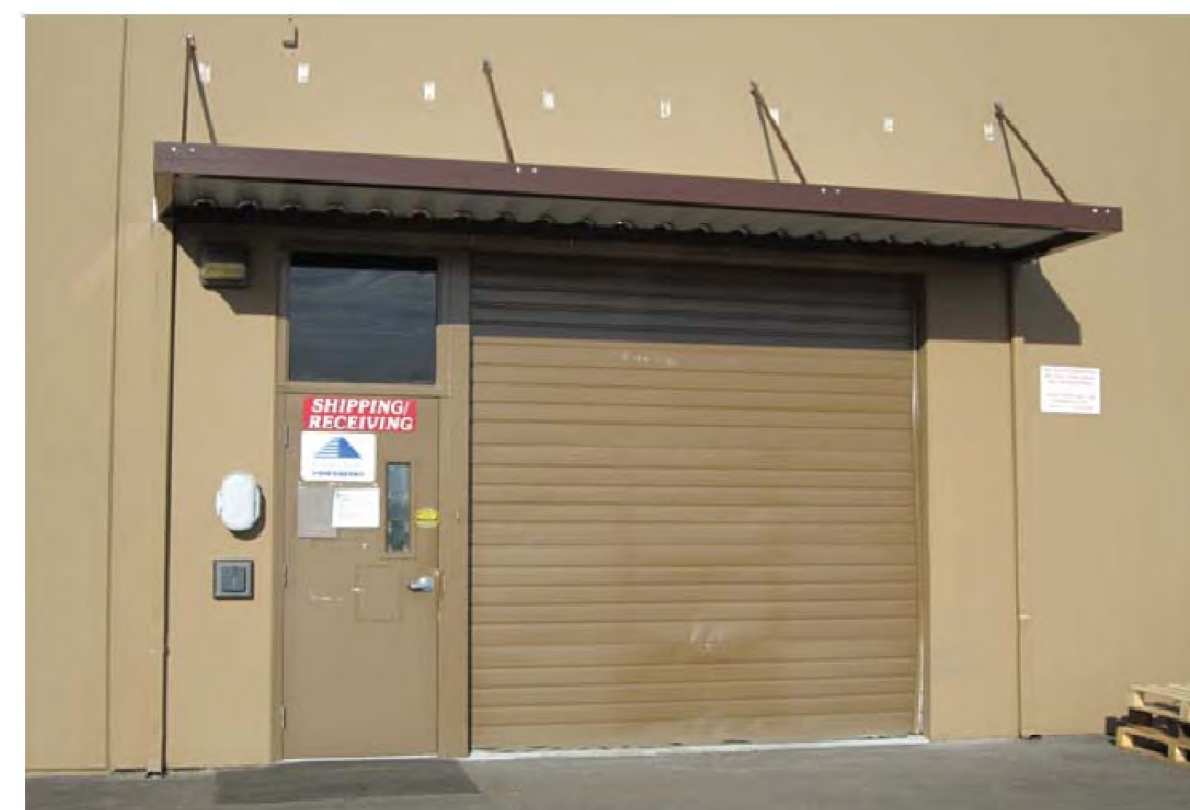
LOADING DOCK GUARDRAIL



ADDRESS NUMERALS



LOADING DOCK DOOR SEALS



OVERHEAD DOOR METAL CANOPY



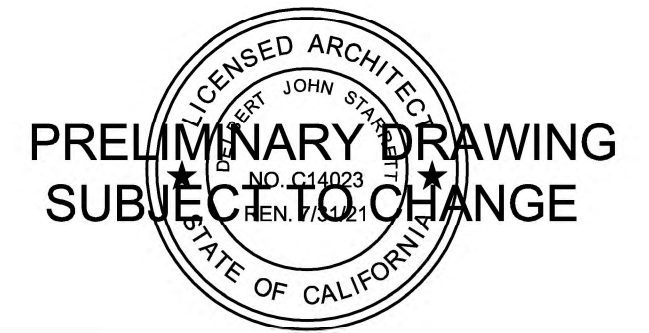
BIKE RACK



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CAD DWG FILE: A50
DRAWN BY: KCT
SCALE: NONE

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SHEET TITLE

SITE FEATURES

SHEET NUMBER

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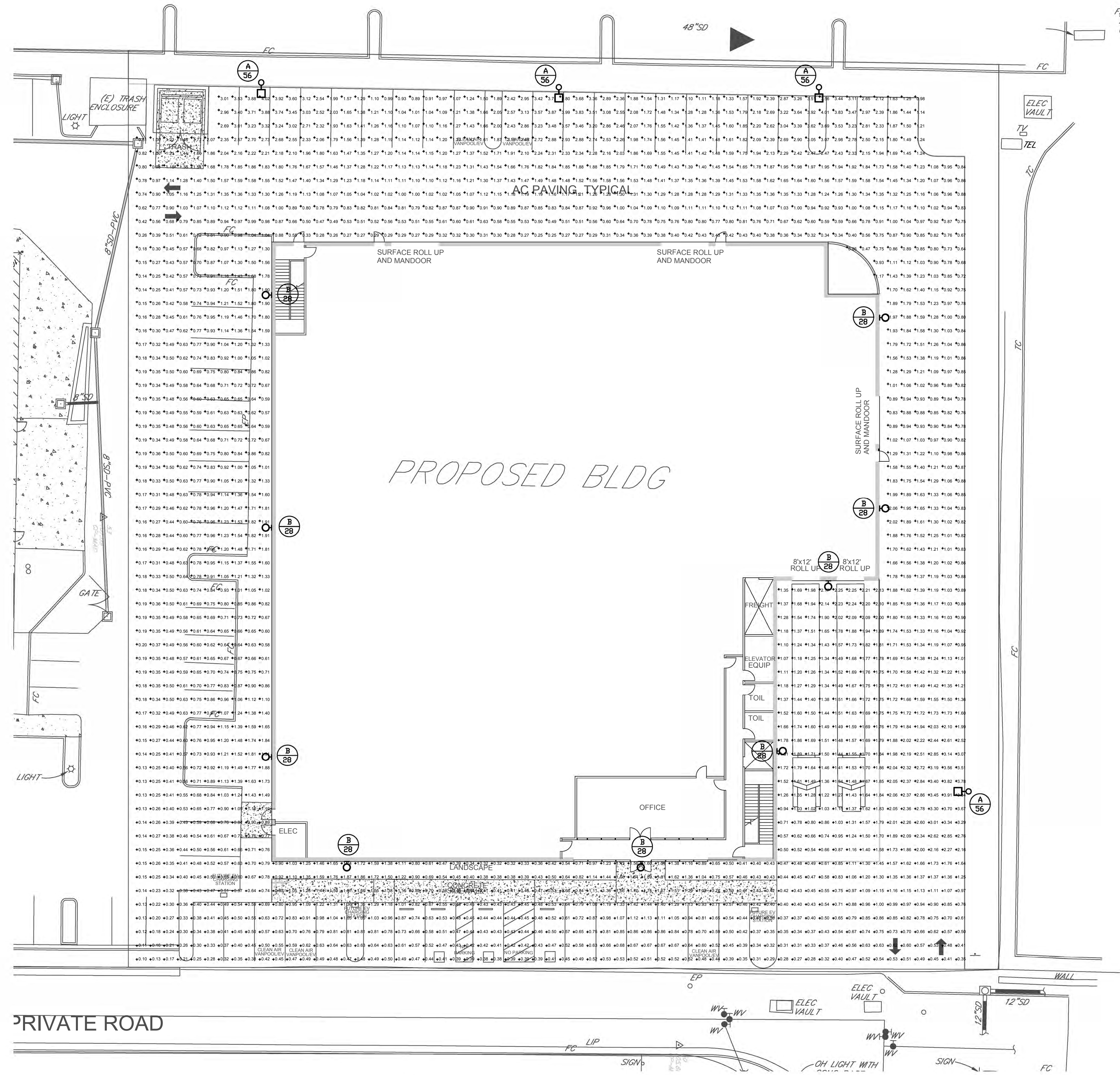
LIGHTING FIXTURE SCHEDULE

TAG	MANUFACTURER	DESCRIPTION	LAMPS	WATTS	COLOR	MOUNTING
A	GARDCO ECF-S-32L-400-AM-G2-AR-3-UNV-DD-CM50-MRG-BZ SSSS-1417-D1	14' POLE LIGHT WITH MOTION SENSOR AND DIMMING SINGLE HEADED POLE	LED	56	DARK BRONZE	POLE
B	GARDCO 101L-16L-530-NW-G1-3-EBPC-UNV-DD-BZ	WALL MOUNTED SCONCE	LED	28	DARK BRONZE	WALL

ELECTRICAL SYMBOLS LEGEND

NOTE: ALL OF THESE SYMBOLS MAY NOT NECESSARILY APPEAR ON THE DRAWINGS
DASHED LINETYPE INDICATES EXISTING DEVICE OR EQUIPMENT

- FIXTURE TAG TYPE 'A', 90-VOLT AMPERES
- FLUORESCENT FIXTURE, RECESSED
- FLUORESCENT FIXTURE, SURFACE
- FLUORESCENT FIXTURE, WALL MOUNTED
- FLUORESCENT INDUSTRIAL OR OPEN STRIP
- POLE LIGHT
- WALL LIGHT
- RECESSED CAN LIGHT
- DETAIL TAG DETAIL #1, SHEET E11
- GROUNDING ELECTRODE OR CONNECTION
- AFF ABOVE FINISHED FLOOR (TO CENTER LINE)
- AFG ABOVE FINISHED GRADE (TO CENTER LINE)
- CO CONDUIT ONLY (PROVIDE PULL-LINE)
- EMS ENERGY MANAGEMENT SYSTEM
- (E) EXISTING
- GRD GROUND
- GFI GROUND FAULT INTERRUPTER
- IG ISOLATED GROUND
- PBO PROVIDED BY OTHERS
- TYP. TYPICAL
- UDN UNLESS OTHERWISE NOTED
- WP WEATHERPROOF



ISSUE	DESCRIPTION	DATE

NEILMED WAREHOUSE
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SYMBOL LEGEND, FIXTURE SCHEDULE AND POINT-BY-POINT

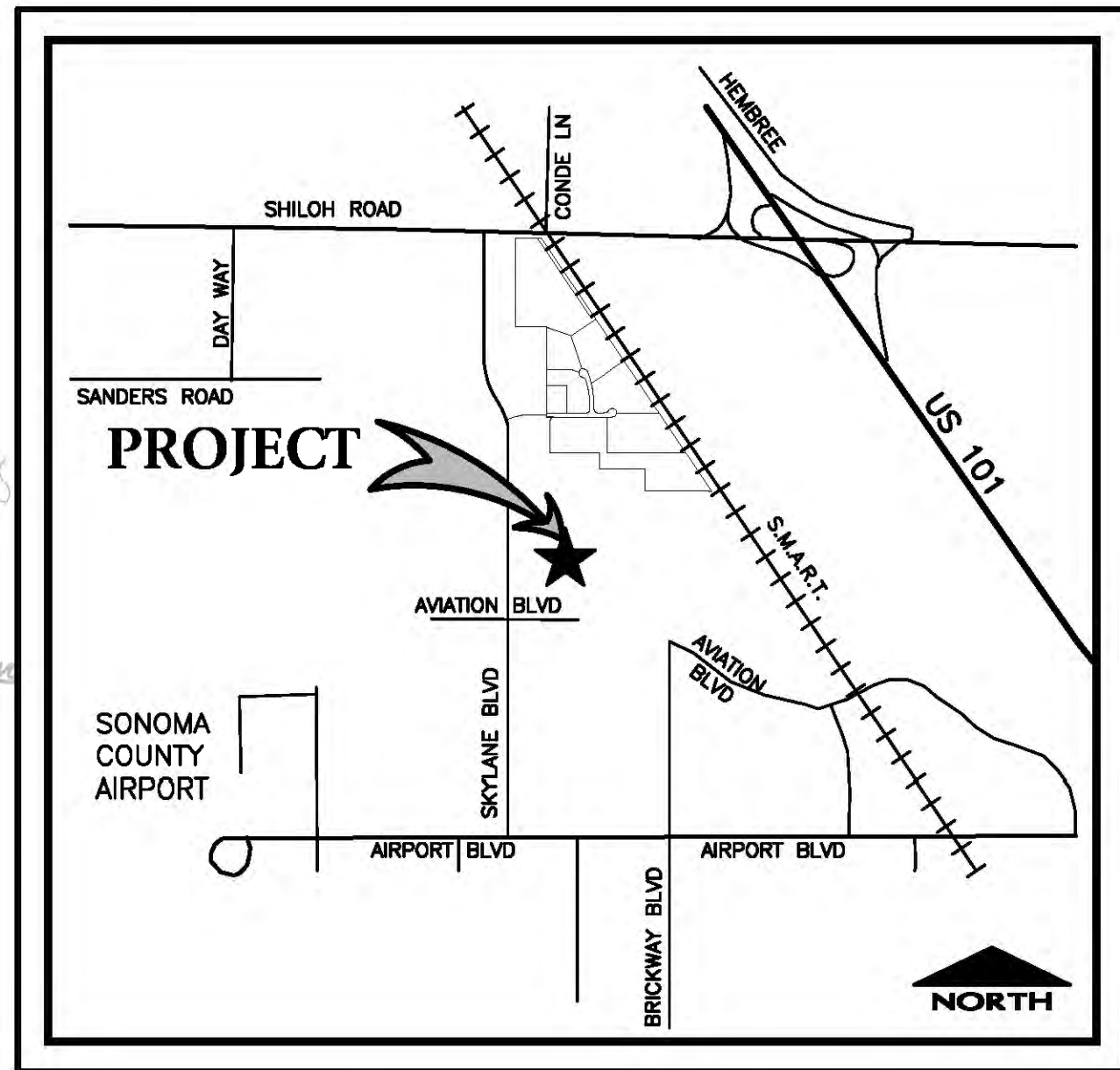
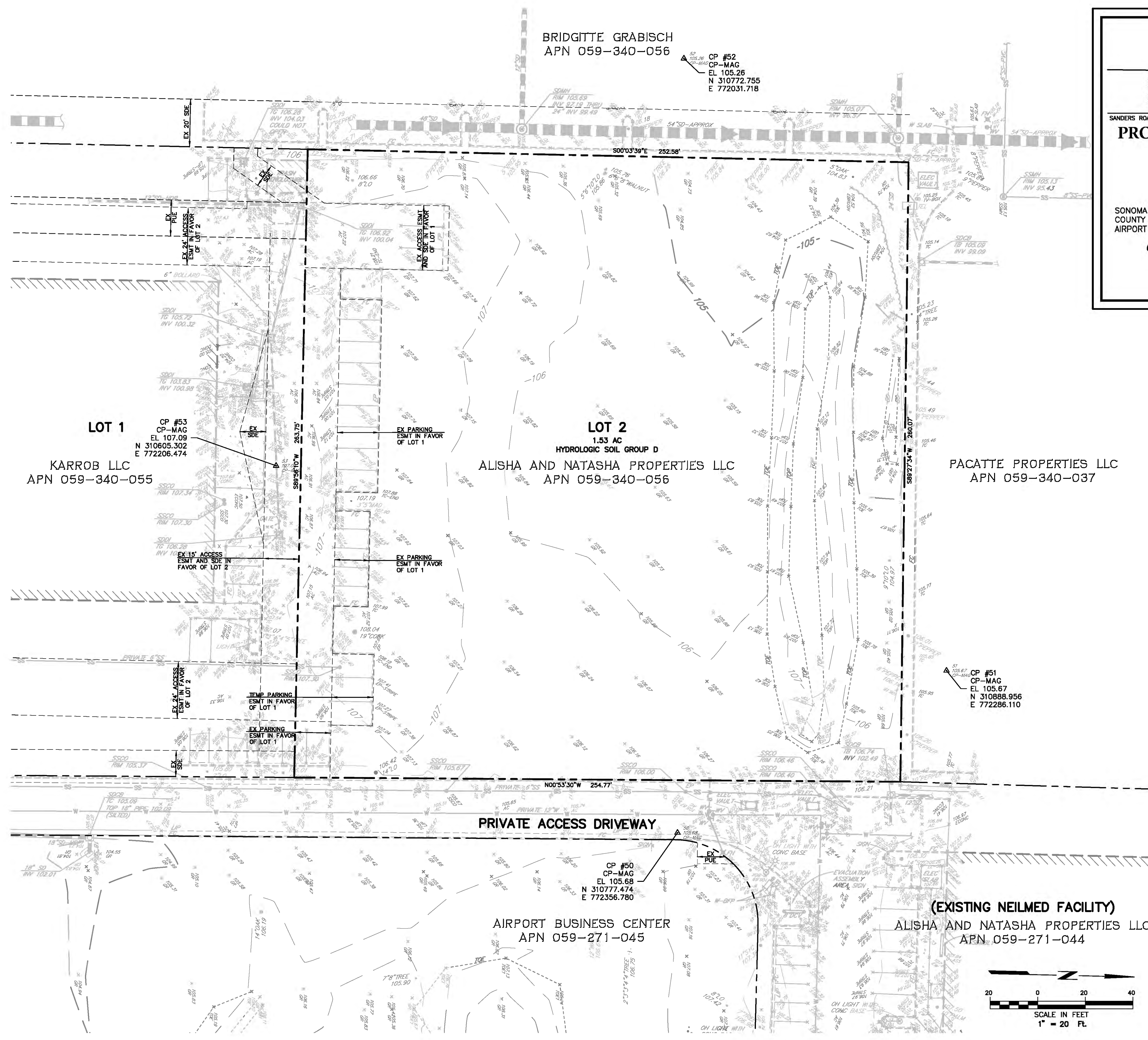
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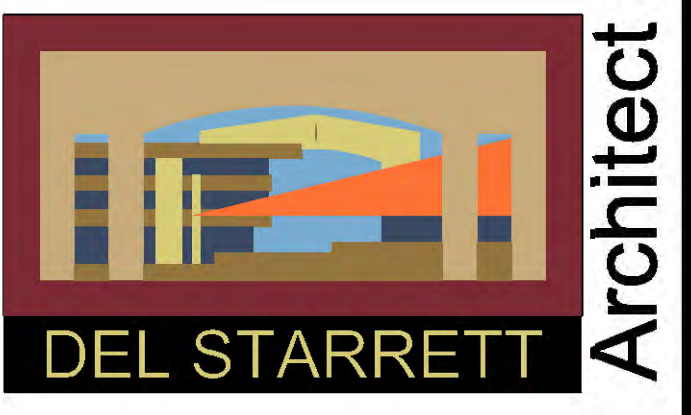
POINT-BY-POINT
SCALE: 1/16" = 1' - 0"

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08-21-19 bailey_v4542.dwg\4542_01_V4542.01_Plan.dwg TAB: C1 - EXISTING CONDITIONS

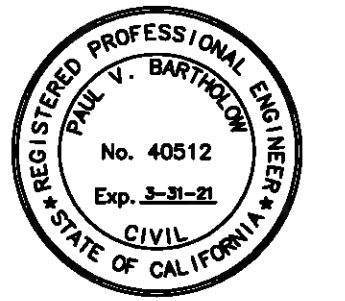


LOCATION MAP
NOT TO SCALE



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v: 707-576-1322
f: 707-576-0469
www.brce.com



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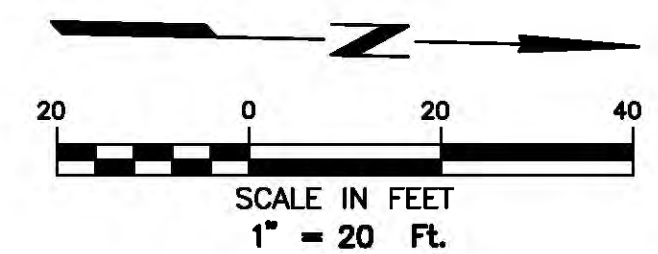
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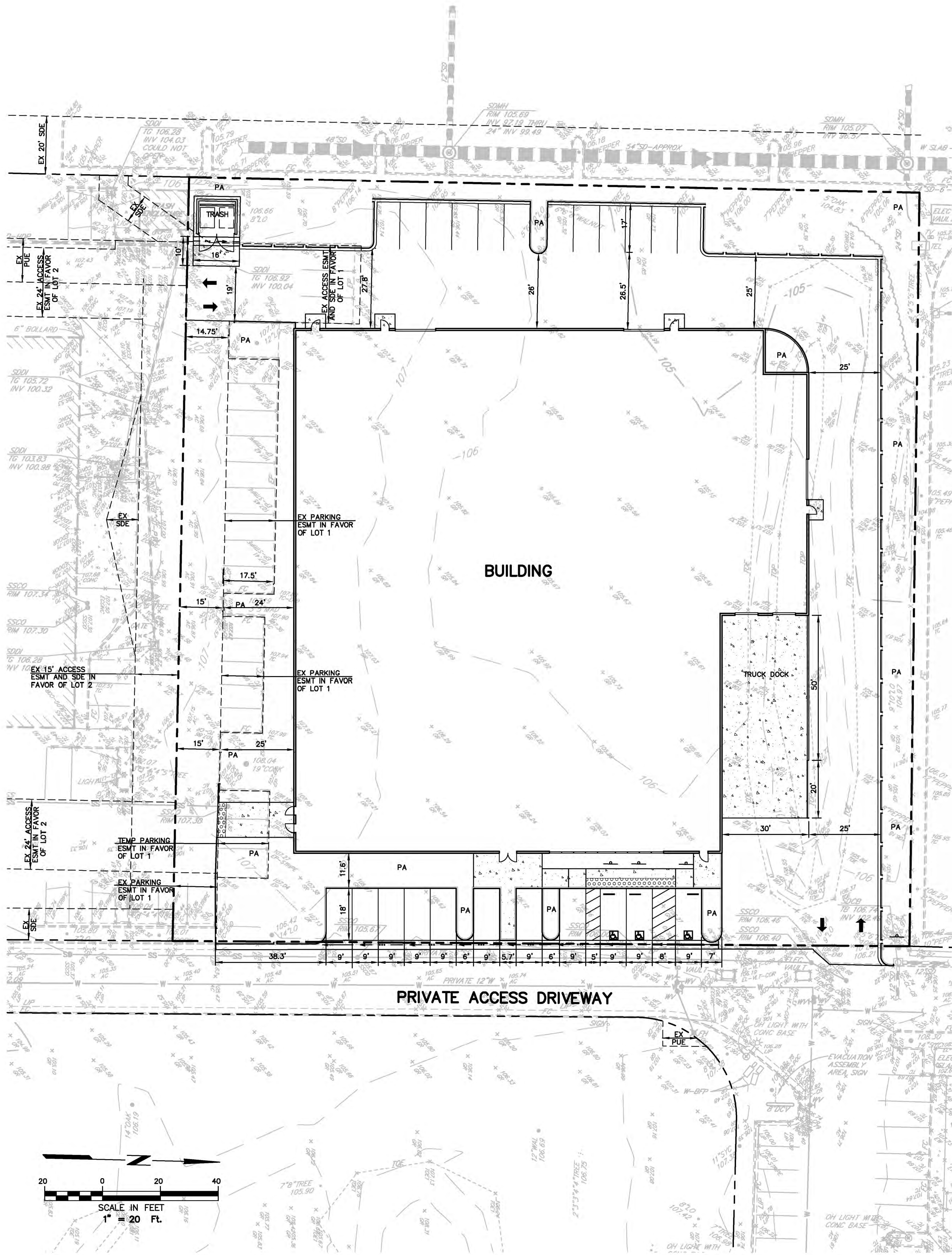
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SHEET TITLE
EXISTING CONDITIONS
EXHIBIT

SHEET NUMBER
C1





ABBREVIATIONS

AB	AGGREGATE BASE	MIN	MINIMUM
AC	ASPHALT CONCRETE	MISC	MISCELLANEOUS
ACP	ASBESTOS CEMENT PIPE	MJ	MECHANICAL JOINT
AD	ALGEBRAIC DIFFERENCE	MSL	MEAN SEA LEVEL
ADA	AMERICANS WITH DISABILITIES ACT	N	NORTH
ADPT	ADAPTER	NO.	NUMBER
AGG	AGGREGATE	NA	NOT APPLICABLE
ALUM	ALUMINUM	NIC	NOT IN CONTRACT
AP	ANGLE POINT	NPT	NATIONAL PIPE THREAD
APN	ASSESSORS PARCEL NUMBER	OC	ON CENTER
APPROX	APPROXIMATE	OD	OUTSIDE DIAMETER
ARV	AIR RELEASE VALVE	OH	OVERHEAD
AVE	AVENUE	OZ	OUNCE
BC	BEGIN HORIZONTAL CURVE	PA	PLANTER AREA
BLDG	BUILDING	PC	POINT OF CURVATURE
BLVD	BOULEVARD	PCC	POINT OF COMPOUND CURVATURE
BM	BENCHMARK	PCC	PORTLAND CEMENT CONCRETE
BO	BLOWOFF	PD	PLANTER DRAIN
BV	BUTTERFLY VALVE	PE	PLAIN END
BVC	BEGIN VERTICAL CURVE	PG	PAD GRADE
BSW	BACK OF SIDEWALK	PVI	POST INDICATOR VALVE
BT	BOTTOM OF TAPER	R	PROPERTY LINE
B&R	BRELJE & RACE	PN	PAVING NOTCH
CAV	COMBINATION AIR AND VACUUM RELEASE VALVE	POC	POINT OF CONNECTION
CB	CATCH BASIN	POC	POINT ON CURVE
CB	CALIFORNIA BUILDING CODE	POS	PRIVATE OPEN SPACE
CD	CONTROLLED DENSITY FILL	POT	POINT ON TANGENT
CHK	CHECK	PP	POWER POLE
CHP	CAST-IN-PLACE PIPE	PRC	POINT OF REVERSE CURVATURE
CL	CENTERLINE	PRV	PRESSURE REDUCING VALVE
CLR	CLASS	PS	PER SQUARE INCH
CMP	CORRUGATED METAL PIPE	PSV	PRESSURE SUSTAINING VALVE
CMPA	CORRUGATED METAL PIPE ARCH	PT	POINT OF TANGENCY
CO	CLEANOUT	PUE	PUBLIC UTILITY EASEMENT
COX	COAXIAL CABLE	PVC	POLYVINYL CHLORIDE
CONC	CONCRETE	PVI	POINT OF VERTICAL INTERSECTION
COND	CONDUIT	PWT	PAVEMENT
COTG	CLEANOUT TO GRADE	R	RADIUS
CP	CONTROL POINT	RAW	RAW WATER
CPLG	COUPLING	RCB	RELATIVE COMPACTION
CR	CURB RETURN	RC	REINFORCED CONCRETE BOX
CSP	CORRUGATED STEEL PIPE	RCP	REINFORCED CONCRETE PIPE
CTR	CENTER	RD	ROAD
CY	CUBIC YARDS	RD	ROOF DRAIN
C/C	CURB TO CENTER	RED	REDUCER
C&G	CURB AND GUTTER	REF	REFERENCE
DBL	DOUBLE	RPBP	REDUCED PRESSURE BACKFLOW PREVENTER
DDCC	DOUBLE CHECK DETECTOR CHECK	RSC	REMOTE SUPERVISORY CONTROL
DDC	DOUBLE DETECTOR CHECK	RT	RIGHT
DET	DETECTOR	RT	RING TIGHT
DI	DROP INLET	RW	RECYCLED WATER
DIA	DIAMETER	RWL	RAIN WATER LEADER
DIP	DUCTILE IRON PIPE	R/W	RIGHT OF WAY
DR	DRIVE	S	SOUTH
DS	DOWNSPOUT	S	SLOPE
DS	DOWNSTREAM	S.A.D.	SEE ARCHITECTURAL DRAWINGS
DWG	DRAWING	SCADA	SUPERVISORY CONTROL AND DATA ACQUISITION
D/W	DRIVEWAY	SCH	SCHEDULE
E	EAST	SD	STORM DRAIN
EC	END HORIZONTAL CURVE	SDCB	STORM DRAIN CATCH BASIN
ECC	ECCENTRIC	SDCO	STORM DRAIN CLEANOUT
EFFL	EFFLUENT (SEWER)	SDDI	STORM DRAIN DROP INLET
EG	EXISTING GROUND	SD	STORM DRAIN EASEMENT
EL	ELEVATION	SDMH	STORM DRAIN MANHOLE
ELEC	ELECTRICAL	SE	SEWER EASEMENT
ELL	ELBOW	S.E.D.	SEE ELECTRICAL DRAWINGS
EP	EDGE OF PAVEMENT	SF	SQUARE FEET
ESMT	EASEMENT	SG	SUBGRADE
EVC	END VERTICAL CURVE	S.L.D.	SEE LANDSCAPE DRAWINGS
EW	EACH WAY	SLP	SLIP ON FLANGE
EX	EXISTING	SO	SIDE OPENING (SD)
EXIST	EXISTING	S.P.D.	SEE PLUMBING DRAWINGS
FC	FACE OF CURB	SPEC	SPECIFICATION
FGA	FLANGED COUPLING ADAPTER	SS	STAINLESS STEEL
FDC	FIRE DEPARTMENT CONNECTION	SS	SANITARY SEWER
FES	FLARED END SECTION	SSCO	SANITARY SEWER CLEANOUT
FF	FINISHED FLOOR	S.S.D.	SEE STRUCTURAL DRAWINGS
FG	FINISHED GRADE	SSMH	SANITARY SEWER MANHOLE
FH	FIRE HYDRANT	ST	STREET
FL	FLOWLINE	STA	STATION
FLG	FLANGE	STD	STANDARD
FLSO	FLOWLINE OF SIDE OPENING	STL	STEEL
FLEX	FLEXIBLE	SVC	SERVICE
FM	FORCE MAIN (PRESSURE)	SWE	SIDEWALK EASEMENT
FRP	FIBERGLASS REINFORCED PLASTIC	S'W	SIDEWALK
FT	FEET	T	TANGENT
FTG	FOOTING	TAN	TANGENT
GALV	GALVANIZED	TB	TOP OF BOX
GB	GRADE BREAK	TC	TOP OF CURB
GPM	GALLONS PER MINUTE	TCE	TEMPORARY CONSTRUCTION
GRD	GROUND	TD	TOP OF DIKE
GSP	GALVANIZED STEEL PIPE	TEL	TELEPHONE
GV	GAS VALVE	TEMP	TEMPORARY
GV	GATE VALVE	TF	TOP OF FOUNDATION
HB	HOSE BIB	TG	TOP OF GRATE
HB	HEADER BOARD	THD	THREADED
HDG	HOT DIPPED GALVANIZED	TS	TOP OF SLAB
HDP	HIGH DENSITY POLYETHYLENE	TT	TOP OF TAPER
HP	HIGH POINT	TW	TOP OF WALL
HPC	HIGH PRESSURE GAS	TY	TYPICAL
HWY	HIGHWAY	UFFG	UNDER FLOOR FINISHED GRADE
ICV	IRRIGATION CONTROL VALVE	UNO	UNLESS NOTED OTHERWISE
ID	INSIDE DIAMETER	VC	VERTICAL CURVE
INV	INVERT	W	WEST
IP	IRON PIPE	W	WATER
IPS	IRON PIPE SIZE	WM	WATER METER
IRR	IRRIGATION	WNF	WELD NECK FLANGE
ISA	INTERNATIONAL SYMBOL OF ACCESSIBILITY	WS	WATER SERVICE
JP	JOINT POLE	WT	WEIGHT
JT	JOINT TRENCH	WV	WATER VALVE
L	LENGTH	°	DEGREES
LF	LINEAL FEET	'	MINUTES
LG	LIP OF GARAGE	"	SECONDS
LG	LIP OF GUTTER	Δ	DELTA
LP	LOW POINT	AT	AND
LT	LEFT	#	NUMBER
MAX	MAXIMUM	#	POUNDS
MBGR	METAL BEAM GUARD RAIL	%	PERCENT
MFR	MANUFACTURE		
MG	MILLION GALLONS		
MH	MANHOLE		

LEGEND

LINES	
BOUNDARY	---
PARCEL	.....
CENTER	.....
EASEMENT	-----
UTILITY LINES	
STORM DRAIN	.....
WATER	.....
SEWER	.....
GAS	.....
ELECTRICAL	.....
TELEPHONE	.....
TELEVISION	.....
JOINT TRENCH	.....
TOPOGRAPHY	
DROP INLET	○
DROP INLET WITH SIDE OPENINGS	○
WATER METER	○
WATER VALVE	○
BLOWOFF	○
FIRE HYDRANT	○
THRUST BLOCK	○
GAS METER	○
STORM DRAIN MANHOLE	○
STORM DRAIN CATCH BASIN	○
SEWER MANHOLE	○
SEWER CLEANOUT	○
JOINT POLE	○
LIGHT STANDARD	○
GUY/ANCHOR	○
CURB & GUTTER	○
AC DIKE	○
FENCE	○
CHAIN LINK FENCE	○
DITCH/SWALE	○
MONUMENT	○
TREE PROTECTION	○
TREE TO BE SAVED	○
TREE TO BE REMOVED	○

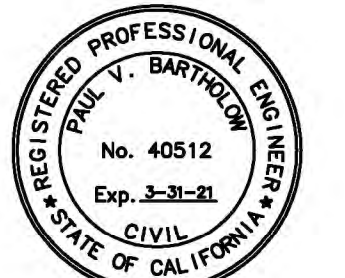
PARKING PROVIDED

STANDARD	12	TOTAL
STANDARD IN ESMT	9	
COMPACT IN ESMT	5	
ADA	3	
FUTURE EV CHARGING	6	
CLEAN AIR VANPOOL/ EV	2	
TOTALS	37	



3663 NORTH LAUGHLIN ROAD, SUITE 207
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 f: 707-576-0469
 www.brce.com



SEAL
PRELIMINARY DRAWING
 SUBJECT TO CHANGE
PRELIMINARY
 NOT FOR CONSTRUCTION
 DATE 8-21-19

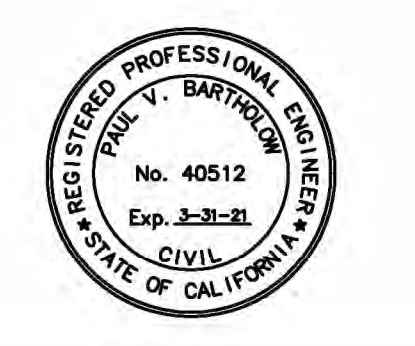
NEILMED
WAREHOUSE
 685 AVIATION BLVD
 SANTA ROSA, CA 95403

REV	DATE	DESCRIPTION
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CAD DWG FILE:	4542.01 PLAN.dwg	
DRAWN BY:	DJK	
SCALE:	1"=20'	

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 Del Starrett Architect

SHEET TITLE
PRELIMINARY SITE PLAN

SHEET NUMBER
C2



REV	DATE	DESCRIPTION

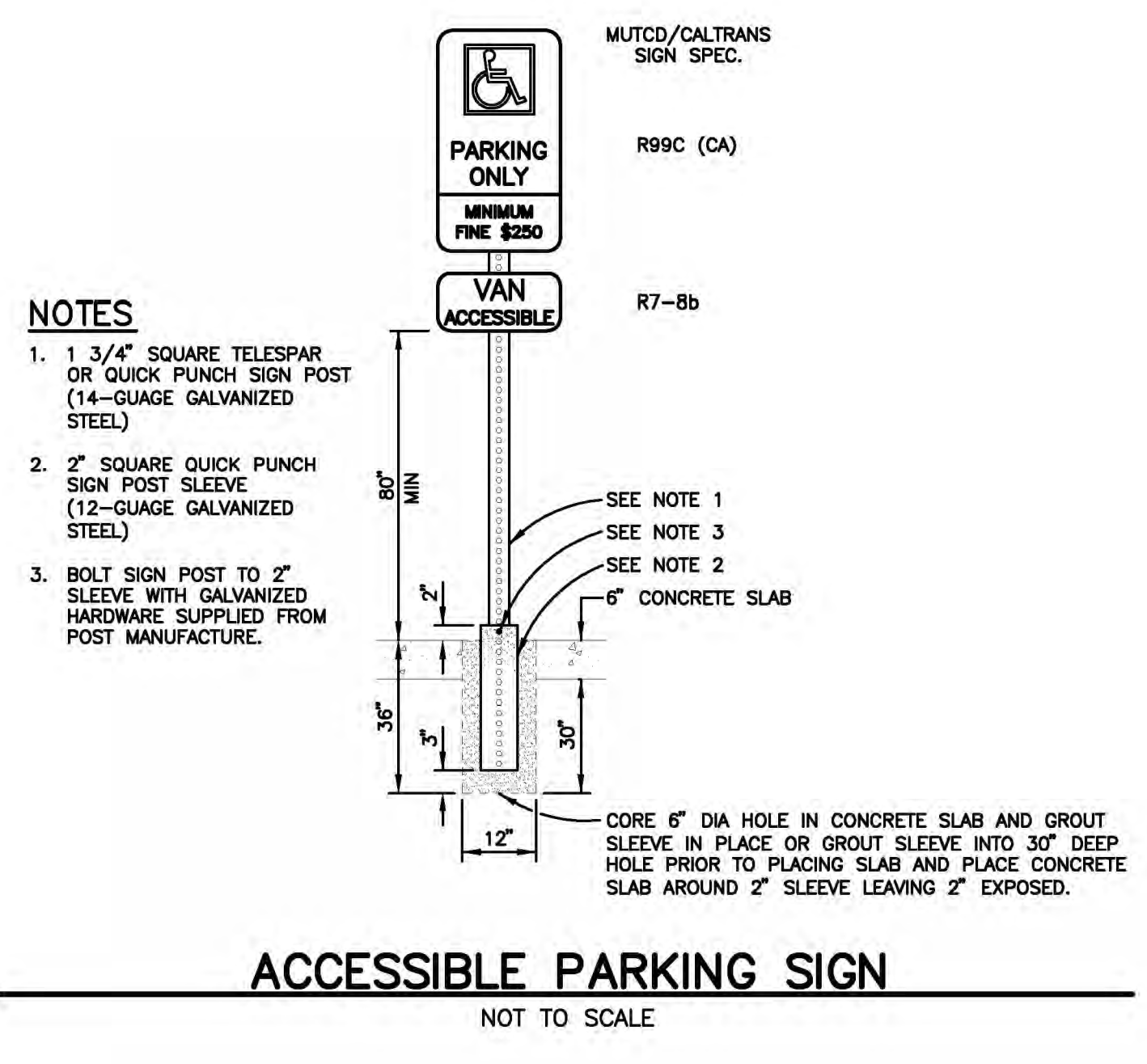
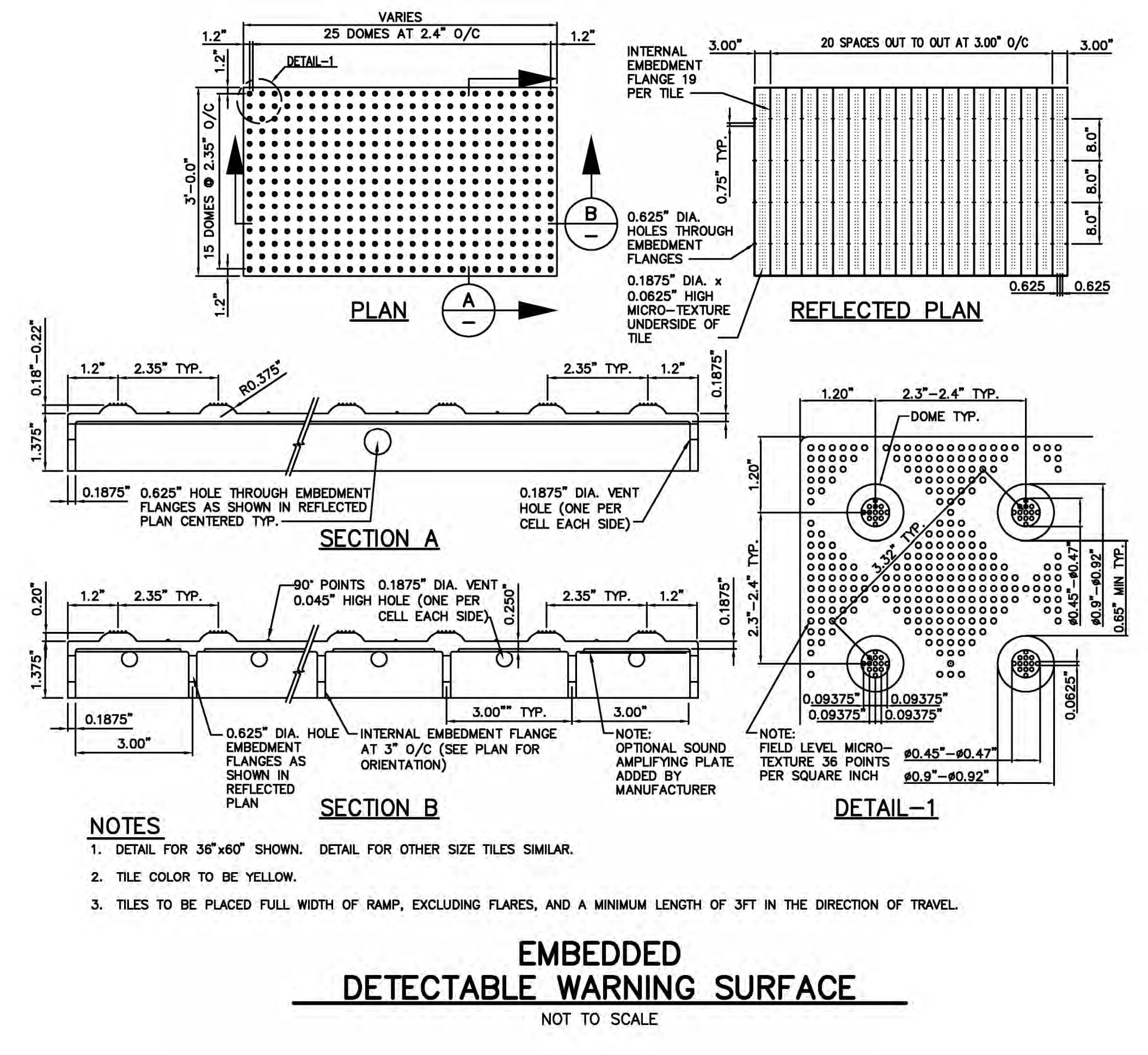
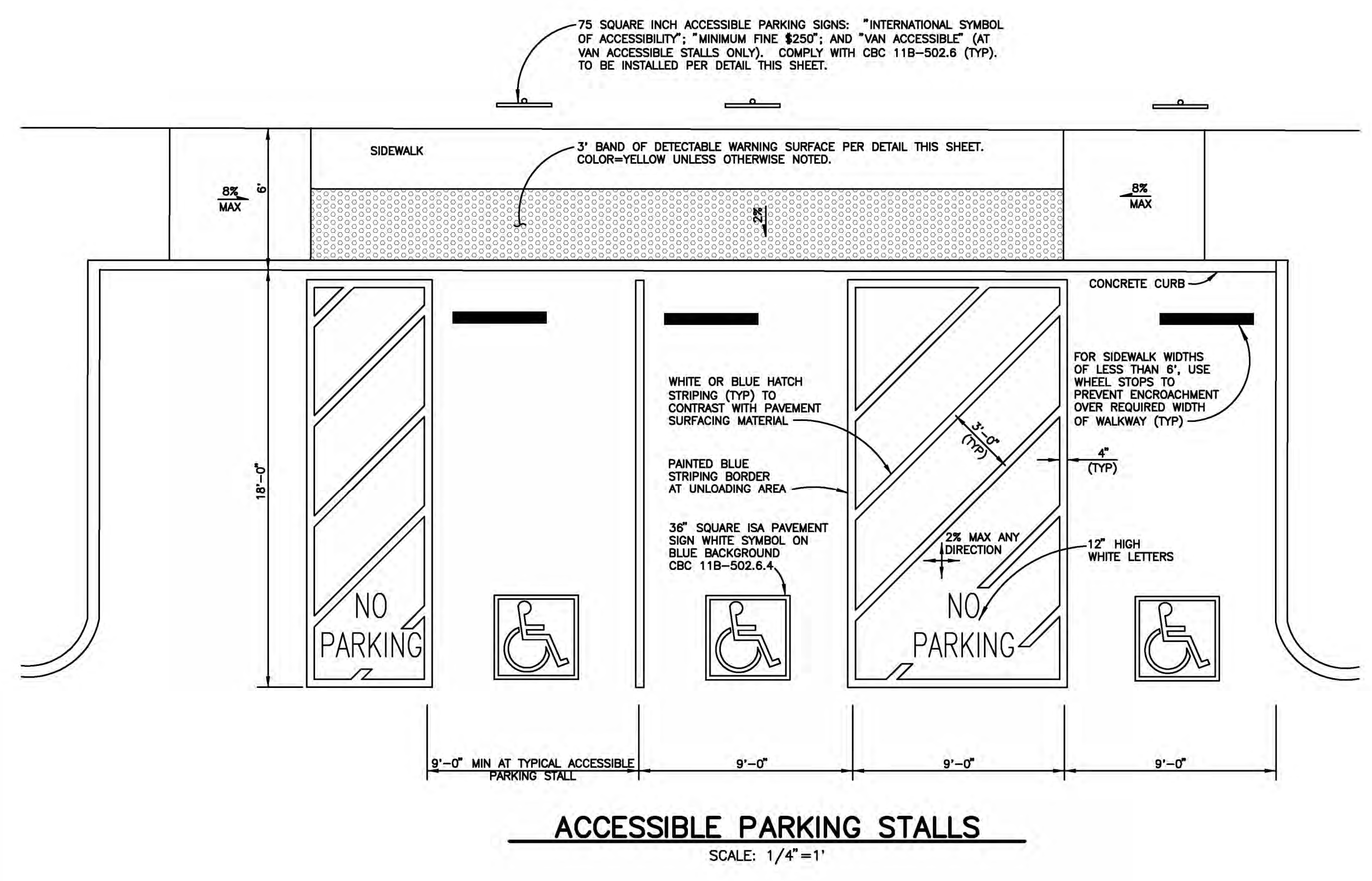
PROJECT NO:	1807/4542.01
CAD DWG FILE:	4542.01 DETAIL.dwg
DRAWN BY:	DJK
SCALE:	AS SHOWN

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SHEET TITLE

DETAILS

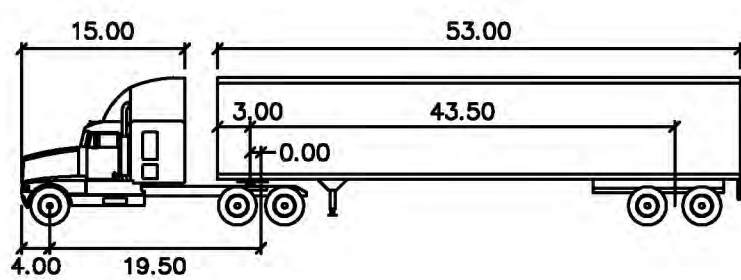
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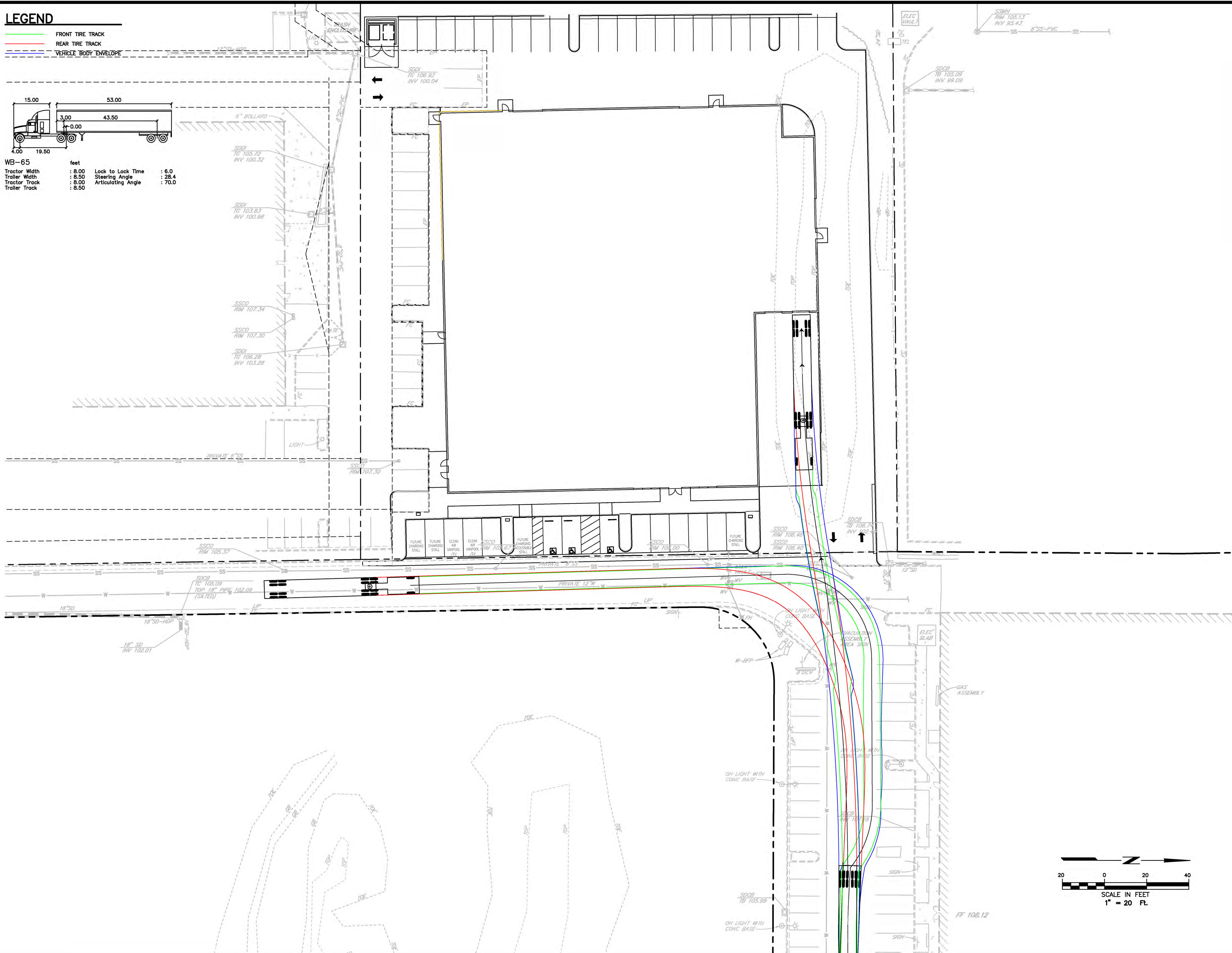
TAB: C7 - DETAILS
 08-21-19
 bailey_4542.dwg
 4542_01_4542.01 DETAIL.dwg

LEGEND

- FRONT TIRE TRACK
- REAR TIRE TRACK
- VEHICLE BODY ENVELOPE



WB-65		feet	
Tractor Width	: 8.00	Lock to Lock Time	: 6.0
Trailer Width	: 8.50	Steering Angle	: 28.4
Tractor Track	: 8.00	Articulating Angle	: 70.0
Trailer Track	: 8.50		



201 CONCOURSE BLVD., SUITE A
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SEAL

PRELIMINARY DRAWING
SUBJECT TO CHANGE

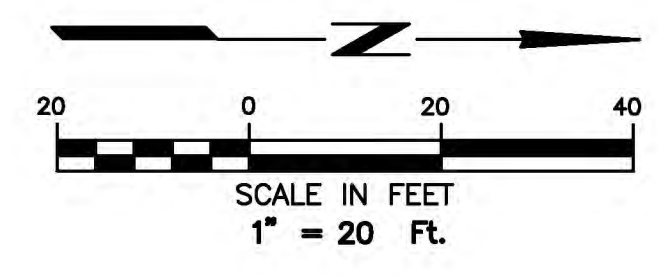
**NEILMED
WAREHOUSE**
685 AVIATION BLVD
SANTA ROSA CA 95403

REV	DATE	DESCRIPTION
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DRAWN BY:	DJK	
SCALE:	1" = 20'	
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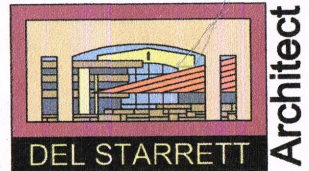
SHEET TITLE

**WB-65 TRUCK
TURNING
TEMPLATE**

SHEET NUMBER

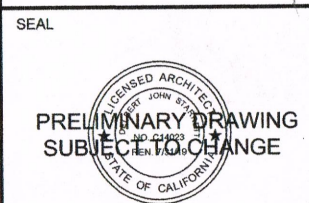


05-21-19 bailey_4542.dwg 4542.00 AUTOTURN=180809--WB65 & Windsor F.t.dwg TAB: WB-65-Entry



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CONSULTANTS



**NEILMED
 WAREHOUSE**
 685 AVIATION BLVD
 SANTA ROSA CA 95403

REV	DATE	DESCRIPTION
6/3/19		DESIGN REVISION
4/11/19		DESIGN REVIEW SUBMITTAL

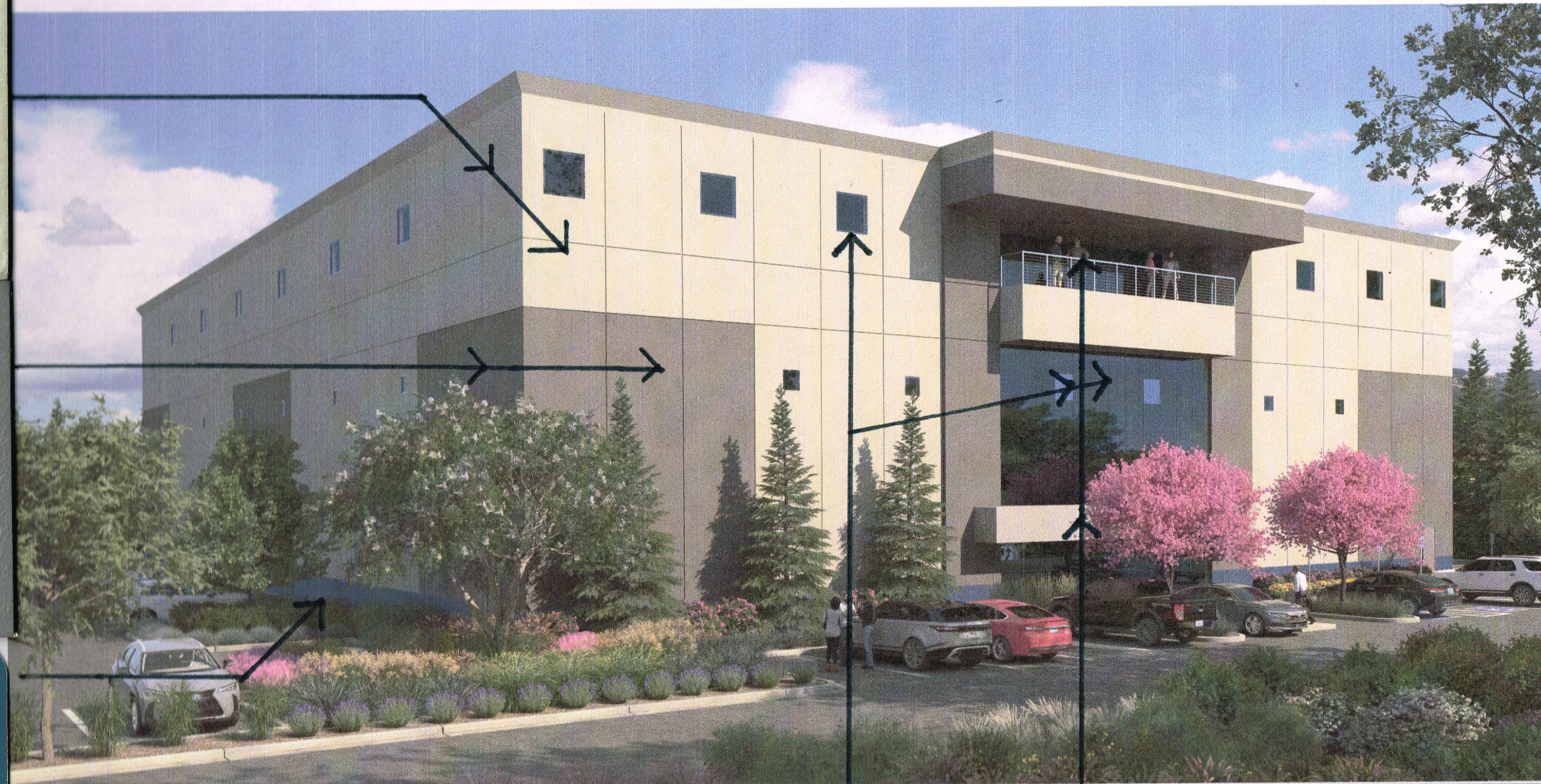
PROJECT NO: 1807
 CAD DWG FILE: A30
 DRAWN BY: KCT
 SCALE: 1/16" = 1'-0"

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SHEET TITLE

ELEVATIONS

SHEET NUMBER
A3.0



Tex-cote over precast concrete panels
 Benjamin Moore - Nantucket Gray

A

Tex-cote over precast concrete panels
 Benjamin Moore - Slate Gray

B

Tex-cote over precast concrete panels
 Pacific Blue at base band around building

C

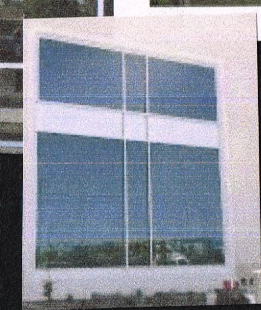
Solarban 70XL
 Solar Blue Vision and Spandrel Glass

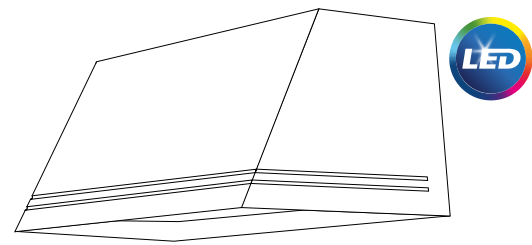
E

Stainless Steel guardrail and Cables

D

Clear aluminum/Silver Metallic
 Facia panels





The 110 Line combines high output and full cutoff performance with design and construction worthy of landmark architecture. The 111 Mini Sconce mirrors the renowned Gardco 100 Line, with classic styling, intelligent engineering and integrity of construction. This compact luminaire provides full cutoff performance without the high angle brightness associated with refractor type products, making them an attractive choice for controlled illumination at points of entry. Add to this seamless material transitions, flawless finishes and engineering considerate of installation, service and long term operation and one immediately appreciates that size need not compromise quality. 111 luminaires installed in the normal downlight position, with a flat glass lens, provide full cutoff performance.

Project:	_____
Location:	_____
Cat.No:	_____
Type:	_____
Lamps:	_____ Qty: _____
Notes:	_____

Ordering Guide

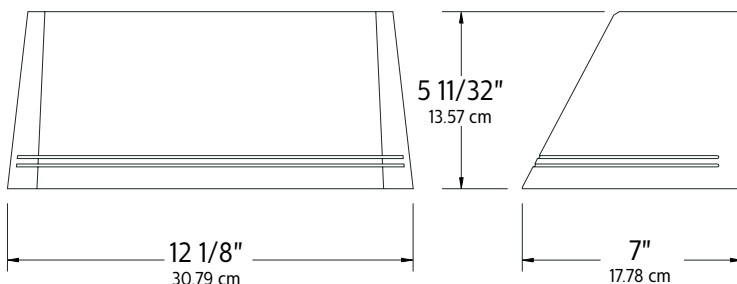
example: 111FT50MH120BRPF

Prefix	Distribution	Wattage	Voltage	Finish	Options
111 Trapezoidal Wedge (Standard Luminaire)	FT Forward Throw T6 MH and CMHE types only	50MH 70MH 35HPS	120 208 240	BRP Bronze Paint BLP Black Paint WP White Paint	F³ Fusing PCB⁴ Button Type Photocontrol SL Solite® Diffusing Lens
111EM Emergency Sconce	WT Wide Throw T6 MH and CMHE types only	50HPS 70HPS 26QF¹	277 347 480	NP Natural Aluminum Paint BGP Beige Paint	UT 5° Uptilt WLU⁵ Wet Location Door for Inverted Mount WG⁶ Wire Guard
111EMC Emergency Sconce, Cold Temperature	MT Medium Throw Fluorescent and E-17 HID lamps only	32TRF¹ 42TRF¹ INC T39MH T70MH T39CMHE¹ T70CMHE¹		OC Optional Color Paint Specify RAL designation ex: OC-RAL7024. SC Special Color Paint Specify. Must supply color chip.	WS⁷ Wall Mounted Box for Surface Conduit EMR LUMINAIRES ONLY⁸ B84CG Bodine Remote Emergency Pack
111EMR Remote Emergency Sconce <i>Refer to configuration chart below for available combinations.</i>					

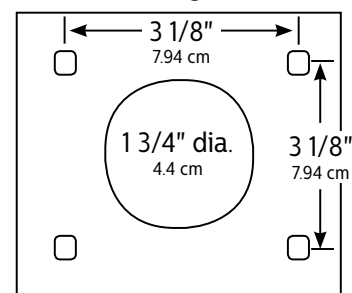
Enter the order code into the appropriate box above. Note: Gardco reserves the right to refuse a configuration. Not all combinations and configurations are valid. Refer to notes below for exclusions and limitations. For questions or concerns, please consult the factory.

- Fluorescent and CMHE luminaires feature electronic ballasts that accept 120V through 277V, 50hz to 60hz, input. Specify "UNIV" voltage for 120V through 277V.
- Not Available in 111EM or 111EMC.
- 120V through 277V only. Not available in Incandescent.
- Not available with 111EM.
- Not Available with WG option.
- Not Available with WLU option.
- Rear entry permitted.
- Emergency Battery Packs for EMR types MUST be ordered with luminaires and supplied by Gardco.

Dimensions



Mounting Plate



Mounting Bolt Pattern

Note: Mounting plate center is located in the center of the luminaire width and 2.38" (6.03cm) above the luminaire bottom (lens down position). Splices must be made in the J-box (by others). Mounting plate must be secured by max. 5/16" (.79cm) diameter bolts (by others) structurally to the wall.

110 Line 111 Mini Sconce

Lamp/Voltage Chart – 111

FT / WT Distribution						
Voltage:	120	208	240	277	347	480
T6 Lamps (T6 Lamps Supplied by Gardco)						
T39MH	▲			▲		
T70MH	▲	▲	▲	▲	▲	
T39CMHE ¹	UNIV				▲	
T70CMHE ¹	UNIV				▲	▲

MT Distribution						
Voltage:	120	208	240	277	347	480
E17 - HID*						
50MH	▲			▲		
70MH	▲	▲	▲	▲	▲	
35HPS	▲					
50HPS	▲			▲		
70HPS	▲	▲	▲	▲	▲	
Fluorescent						
26QF ¹	UNIV					
32TRF ¹	UNIV					
42TRF ¹	UNIV					
Incandescent						
INC	▲					
<i>Combinations marked with a dot, shown with "UNIV" or "200-277" are available for ordering.</i>						
* MH, CMHE and HPS types require medium based E17 lamps. All MH and CMHE types shown are pulse start by design.						
1. Fluorescent and CMHE luminaires feature electronic ballasts that accept 120V through 277V, 50hz to 60hz, input. Specify "UNIV" voltage for 120V through 277V.						

Configuration Chart – 111EM, 111EMC, or 111EMR

	Distribution			Voltage					
	FT	WT	MT	120	208	240	277	347	480
Fluorescent									
26QF			▲	▲			▲		
32TRF			▲	▲			▲		
42TRF ²			▲	▲			▲		
<i>MH - Metal Halide CMHE - Ceramic Metal Halide with Electronic Ballast HPS - High Pressure Sodium TRF - Triple Tube Fluorescent QF - Quad Fluorescent</i>									
<i>Combinations marked with a dot, shown with "UNIV" or "200-277" are available for ordering.</i>									
2. Not Available in 111EM or 111EMC.									

110 Line 111 Mini Sconce

Specifications

GENERAL

Each Gardco 111 luminaire is a wall mounted cutoff luminaire for high intensity discharge, compact fluorescent or incandescent lamps. Internal components are totally enclosed in a rain-tight, dust-tight and corrosion resistant housing. The housing, back plate and door frame are die cast aluminum. A choice of three (3) optical systems is available. Luminaires are suitable for wet locations (damp locations if inverted).

HOUSING

Single-piece soft trapezoidal housings are die cast aluminum. A memory retentive gasket seals the housing with the doorframe to exclude moisture, dust, insects and pollutants from the optical system. A black, die cast ribbed backplate dissipates heat for longer lamp and ballast life.

DOOR FRAME

A single-piece die cast aluminum door frame integrates to the housing form. The door frame is hinged closed and secured to the housing with two (2) captive stainless steel fasteners. The heat and impact resistant 1/8" (.32cm) tempered glass lens and one-piece gasket are mechanically secured to the door frame.

OPTICAL SYSTEMS

Reflectors are composed of specular extruded and faceted components, electropolished, anodized and sealed. Reflector segments are set in arc tube image duplicating patterns to achieve the wide throw, forward throw or medium throw distributions.

ELECTRICAL

Standard Luminaires: Each high power factor HID core and coil or electronic ballast is capable of providing reliable lamp starting down to -20°F/-29°C. Component-to-component wiring within the luminaire will carry no more than 80% of rated current and is listed by UL for use at 600 VAC at 302°F/150°C or higher. Plug disconnects are listed by UL for use at 600 VAC, 15A or higher.

Standard fluorescent units have a starting temperature of 0°F/-18°C. Standard fluorescent ballasts are high power factor electronic solid state.

EM and EMC Luminaires: In the event of power interruption, integral battery pack will power (1) 32W or (1) 26W compact fluorescent lamp at reduced light levels for a minimum of 90 minutes.

Maintenance free battery is rated for ambient temperatures down to 32°F/0°C (EM) or -4°F/-20°C (EMC). Indicator light is visible through the lens. A test switch is accessible through the door assembly. EM and EMC units do not bear CUL label.

EMR Luminaires: A 7.5'/2.29m, 12 wire, quick disconnect assembly is provided for wiring through conduit (by others) to a Bodine B84CG fluorescent emergency battery pack. The fluorescent emergency battery pack MUST be supplied by Gardco. The B84CG option is required to be on the order to the factory. In the event of power interruption, the B84CG remote battery pack will power (1) 42W, (1) 32W or (1) 26W compact fluorescent lamp at at reduced light levels for a minimum of 90 minutes.

Maintenance free battery is rated for ambient temperatures down to 32°F/0°C. Minimum luminaire starting temperature is 0°F/-18°C. Indicator light is visible through the lens. A test switch is accessible through the door assembly. NOTE: B84CG does not bear CUL label.

LAMPHOLDER

Pulse rated medium base sockets are glazed porcelain with nickel plated screw shell. T6 lamps use a G12 base, pulse rated porcelain sockets. Fluorescent sockets are high temperature PBT with brass contacts.

FINISH

Each standard color luminaire receives a fade and abrasion resistant, electrostatically applied, thermally cured, triglycidal isocyanurate (TGIC) textured polyester powdercoat finish. Standard colors include bronze (BRP), black (BLP), white (WP), natural aluminum (NP) and beige (BGP). Consult factory for specs on custom colors.

LABELS

All luminaires bear either UL or CUL (where applicable) Wet Location labels. Lens down application is Wet Location and lens up is Damp Location, except when using the optional inverted Wet Location components.

WARRANTY

Gardco luminaires feature a 5 year limited warranty. See Warranty Information on www.sitelighting.com for complete details and exclusions. Polycarbonate lenses carry a 1 year warranty only.

FULL CUTOFF PERFORMANCE: Full cutoff performance means a luminaire distribution where zero candela intensity occurs at an angle at or above 90° above nadir. Additionally, the candela per 1000 lamp lumens does not numerically exceed 100 (10 percent) at a vertical angle of 80° above nadir. This applies to all lateral angles around the luminaire.

CUTOFF PERFORMANCE: Cutoff performance means a luminaire distribution where the candela per 1000 lamp lumens does not numerically exceed 25 (2.5 percent) at an angle at or above 90° above nadir, and 100 (10 percent) at a vertical angle of 80° above nadir. This applies to all lateral angles around the luminaire.





The Gardco EcoForm Gen-2 luminaire features narrow spectrum amber LEDs combined with special shielding, making it a wildlife friendly lighting solution. EcoForm features an innovative retrofit arm kit, simplifying site conversions to LED by eliminating the need to drill additional holes in most existing poles. Integral control systems available for further energy savings.

Project: _____
 Location: _____
 Cat.No: _____
 Type: _____
 Qty: _____
 Notes: _____

Ordering guide

example: ECF-S-64L-600-AM-G2-AR-5-120-FWC-MGY

Prefix	Number of LEDs	Drive Current	LED Color - Generation	Mounting	Distribution	Voltage	Options					Finish
							Dimming controls	Motion sensing	Photo-sensing	Electrical	Luminaire	
ECF-S			AM-G2									
ECF-S EcoForm site and area, wildlife	32L 32 LEDs (2 modules)	400 400mA	AM-G2 Direct Amber, nominal 590nm narrow spectrum LED Generation 2	AR Arm Mount (standard) ⁵ <i>The following mounting kits must be ordered separately (See accessories)</i> WS Wall mount with surface conduit rear entry permitted RAM Retrofit arm mount kit ⁵	Type 2	120 120V	DD 0-10V External dimming (by others) FAWS Field Adjustable ¹⁰ SW Interface module for SiteWise ^{8,9} DynaDimmer: Automatic Profile Dimming CS50 Safety 50% Dimming, 7 hours ¹ CM50 Median 50% Dimming, 8 hours ¹ CE50 Economy 50% Dimming, 9 hours ¹ DA50 All Night 50% Dimming ¹ CS30 Safety 30% Dimming, 7 hours ¹ CM30 Median 30% Dimming, 8 hours ¹ CE30 Economy 30% Dimming, 9 hours ¹ DA30 All Night 30% Dimming ¹	IMR13 Integral with #3 lens ⁴ IMR17 Integral with #7 lens ⁴	PCB Photocontrol Button ^{2,3} TLRD5 Twist Lock Receptacle 5 Pin ¹¹ TLRD7 Twist Lock Receptacle 7 Pin ¹¹ TLRPC Twist Lock Receptacle w/ Photocell ²	Fusing F1 Single (120, 277, 347VAC) ² F2 Double (208, 240, 480VAC) ² Pole Mount Fusing FP1 Single (120, 277, 347VAC) ² FP2 Double (208, 240, 480VAC) ² FP3 Canadian Double Pull (208, 240, 480VAC) ² Surge Protection (10kA standard) SP2 Increased 20kA	Square Pole Adapter included in standard product TB Terminal Block RPA Round Pole Adapter (fits to 3"- 3.9" O.D. pole) ⁵ FWC Wildlife Friendly External House side shield ¹²	Textured
	48L 48 LEDs (3 modules)	600 600mA			2 Type 2	208 208V						BK Black
	48L 48 LEDs (3 modules)	600 600mA			3 Type 3	240 240V						WH White
	64L 64 LEDs (4 modules)	600 600mA			4 Type 4	277 277V						BZ Bronze
	64L 64 LEDs (4 modules)	600 600mA			5 Type 5	347 347V						MGY Medium Gray
				5W Type 5W	HVU 120-277V (50/60Hz) HVU 347-480V (50/60Hz)				Customer specified	RAL Specify optional color or RAL (ex: RAL7024) CC Custom color (Must supply color chip for required factory quote)		

- Available only on 120, 208, 240, and 277 (or UNV)
- Specify Voltage
- Not available with 347 or 480 voltage
- ECF-IMRI equipped with out-boarded sensor housing when voltage is HVU (347-480V)
- Mounts to a 4" round pole with adapter included for square poles.
- Not available with WS. RPAs provided with black finish standard
- Limited to a maximum of 45 degrees aiming above horizontal
- SW option is not available with any other control options with the exception of IMR13 and IMR17 motion response options
- Available only on 120V and 277V
- Not available with IMR13, IMR17, SW, and CS/CM/CE/DA (DynaDimmer).
- TLRD5/7 option not available with PCB or TLRPC. Max aiming angle 45°. Works with 3 or 5 pin NEMA photocell/dimming. Dimming will not be connected to NEMA receptacle if ordering with DD, CS/CM/CE/DA, and IMRI.
- External shield shipped with luminaire.



ECF-S EcoForm wildlife

Site & Area

EcoForm Accessories (ordered separately, field installed)

Luminaire Accessories

ECF-BD-G2	Bird deterrent
ECF-RAM-G2-(F)	Retrofit Arm mount kit
ECF-WS-G2-(F)	Wall mount with surface conduit rear entry permitted

EcoForm PTF2
(pole top fitter fits 23/8-21/2" OD x 4" depth tenon)

PTF2-ECF-S/L-1-90-(F)	1 luminaire at 90°
PTF2-ECF-S/L-2-90-(F)	2 luminaires at 90°
PTF2-ECF-S/L-2-180-(F)	2 luminaires at 180°
PTF2-ECF-S/L-3-90-(F)	3 luminaires at 90°
PTF2-ECF-S/L-4-90-(F)	4 luminaires at 90°
PTF2-ECF-S/L-3-120-(F)	3 luminaires at 120°

EcoForm PTF3
(pole top fitter fits 3-31/2" OD x 6" depth tenon)

PTF3-ECF-S/L-1-90-(F)	1 luminaire at 90°
PTF3-ECF-S/L-2-90-(F)	2 luminaires at 90°
PTF3-ECF-S/L-2-180-(F)	2 luminaires at 180°
PTF3-ECF-S/L-3-90-(F)	3 luminaires at 90°
PTF3-ECF-S/L-4-90-(F)	4 luminaires at 90°
PTF3-ECF-S/L-3-120-(F)	3 luminaires at 120°

EcoForm PTF4
(pole top fitter fits 31/2-4" OD x 6" depth tenon)

PTF4-ECF-S/L-1-90-(F)	1 luminaire at 90°
PTF4-ECF-S/L-2-90-(F)	2 luminaires at 90°
PTF4-ECF-S/L-2-180-(F)	2 luminaires at 180°
PTF4-ECF-S/L-3-90-(F)	3 luminaires at 90°
PTF4-ECF-S/L-4-90-(F)	4 luminaires at 90°
PTF4-ECF-S/L-3-120-(F)	3 luminaires at 120°

(F) = Specify finish

Predicted Lumen Depreciation Data

Predicted performance derived from LED manufacturer's data and engineering design estimates, based on IESNA LM-80 methodology. Actual experience may vary due to field application conditions. L70 is the predicted time when LED performance depreciates to 70% of initial lumen output. Calculated per IESNA TM21-11. Published L70 hours limited to 6 times actual LED test hours

Ambient Temperature °C	Driver mA	Calculated L70 Hours	L70 per TM-21	Lumen Maintenance % at 60,000 hrs
25°C	up to 600 mA	>100,000 hours	>60,000 hours	>88%

LED Wattage and Lumen Values

Unshielded

Ordering Code	Total LEDs	LED Current (mA)	Dominant Wavelength (nm)	Average System Watts	Type 2			Type 3			Type 4			Type 5			Type SW		
					Lumen Output	Efficacy (LPW)	BUG Rating	Lumen Output	Efficacy (LPW)	BUG Rating	Lumen Output	Efficacy (LPW)	BUG Rating	Lumen Output	Efficacy (LPW)	BUG Rating	Lumen Output	Efficacy (LPW)	BUG Rating
ECF-S-32L-400-AM-G2-x	32	400	596	32	2,092	66	B1-U0-G1	2,047	65	B1-U0-G1	2,141	68	B1-U0-G1	2,259	72	B2-U0-G0	2,187	69	B2-U0-G1
ECF-S-32L-600-AM-G2-x	32	600	596	48	2,449	51	B1-U0-G1	2,396	50	B1-U0-G1	2,507	52	B1-U0-G1	2,645	55	B2-U0-G1	2,560	53	B2-U0-G1
ECF-S-48L-400-AM-G2-x	48	400	596	47	3,120	66	B1-U0-G1	3,052	64	B1-U0-G1	3,193	67	B1-U0-G1	3,369	71	B2-U0-G1	3,261	69	B2-U0-G1
ECF-S-48L-600-AM-G2-x	48	600	596	72	3,650	50	B1-U0-G1	3,571	49	B1-U0-G1	3,736	52	B1-U0-G1	3,943	54	B3-U0-G1	3,816	53	B3-U0-G1
ECF-S-64L-400-AM-G2-x	64	400	596	62	4,106	67	B1-U0-G1	4,017	65	B1-U0-G1	4,202	68	B1-U0-G1	4,434	72	B3-U0-G1	4,292	70	B3-U0-G1
ECF-S-64L-600-AM-G2-x	64	600	596	94	4,805	51	B1-U0-G1	4,701	50	B1-U0-G1	4,918	52	B1-U0-G2	5,190	55	B3-U0-G1	5,023	54	B3-U0-G1

Shielded

Ordering Code	Total LEDs	LED Current (mA)	Dominant Wavelength (nm)	Average System Watts	2-FWC		3-FWC		4-FWC		5-FWC		5W-FWC	
					Lumen Output	Efficacy (LPW)	Lumen Output	Efficacy (LPW)	Lumen Output	Efficacy (LPW)	Lumen Output	Efficacy (LPW)	Lumen Output	Efficacy (LPW)
ECF-S-32L-400-AM-G2-x	32	400	596	32	680	22	665	21	696	22	734	23	710	22
ECF-S-32L-600-AM-G2-x	32	600	596	48	795	16	778	16	814	17	859	18	831	17
ECF-S-48L-400-AM-G2-x	48	400	596	48	1,013	21	991	21	1,037	22	1,094	23	1,059	22
ECF-S-48L-600-AM-G2-x	48	600	596	73	1,186	16	1,160	16	1,213	17	1,281	18	1,239	17
ECF-S-64L-400-AM-G2-x	64	400	596	62	1,334	22	1,305	21	1,365	22	1,441	23	1,394	23
ECF-S-64L-600-AM-G2-x	64	600	596	94	1,561	17	1,527	16	1,598	17	1,686	18	1,632	17

Values from photometric tests performed in accordance with IESNA LM-79 and are representative of the configurations shown. Actual performance may vary due to installation and environmental variables, LED and driver tolerances, and field measurement considerations. It is highly recommended to confirm performance with a photometric layout.

NOTE: Some data may be scaled based on tests of similar (but not identical) luminaires. Contact factory for configurations not shown.

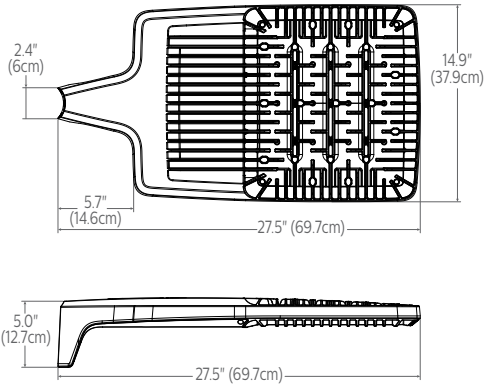
ECF-S EcoForm wildlife

Site & Area

Dimensions

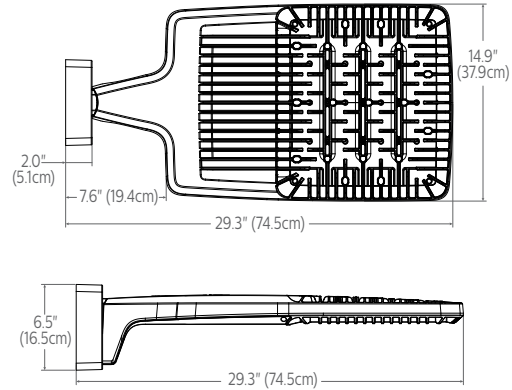
Standard Arm (AR)

Weight: 22 Lbs (9.9 Kg) EPA: 0.21ft² (.019m²)



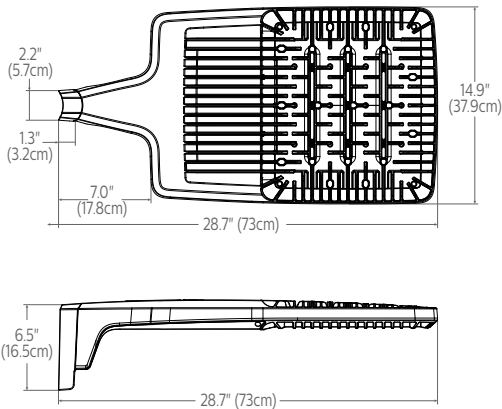
Wall (WS)

Weight: 27 Lbs. (12. 2Kg) EPA: 0.27ft² (.025m²)



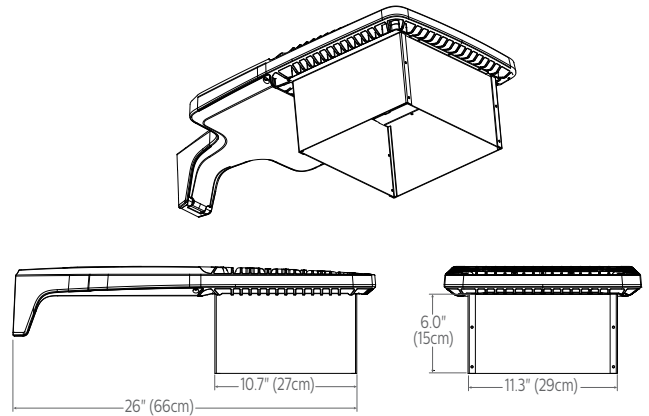
Retrofit Arm (RAM)

Weight: 24 Lbs (10.9 Kg) EPA: 0.24ft² (.022m²)

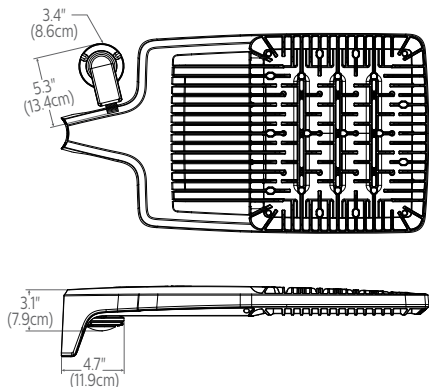


Wildlife Friendly External House side shield (FWC)

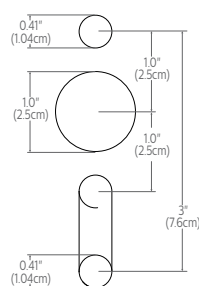
Weight: 23 Lbs (10.4 Kg) EPA: 1.0ft² (.093m²)



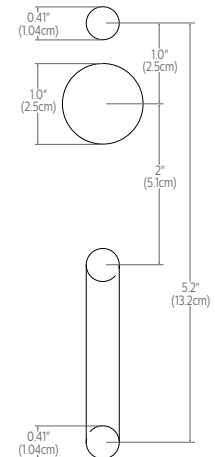
Outboard IMR-HVU sensor



Standard Arm (AR) drill pattern



Retrofit Arm (RAM) drill pattern



ECF-S EcoForm wildlife

Site & Area

Specifications

Housing

One piece die cast aluminum housing with integral arm and separate, self retained hinged, one piece die cast door frame.

IP Rating

LED light engine rated IP66. Driver compartment rated to IP65.

Vibration resistance

Luminaire is tested and rated 3G over 100,000 cycles conforming to standards set forth by ANSI C136.31-2010. Testing includes vibration in three axes, all performed on the same luminaire.

Light Engine

Light engine comprises of a module of 16-LED aluminum metal clad board fully sealed with optics offered in multiples of 2, 3 and 4 modules or 32, 48 and 64 LEDs. Module is RoHS compliant. Direct Amber LED is narrow spectrum with dominant wavelength at 596 nm (peak wavelength at 601 nm). LED light engine is rated IP66 in accordance to Section 9 of IEC 60598-1.

LED Thermal management

The housing design allows the one piece housing to provide excellent thermal management critical to long LED system life.

Optical systems

Type 2, 3, 4, 5 and 5W.

Mounting

Standard luminaire arm mounts to 4" round poles. Can also be used with 5" O.D. poles. Square pole adapter included with every luminaire. Round Pole Adapter (RPA) required for 3-3.9" poles.

Control options

0-10V dimming (DD): Access to 0-10V dimming leads supplied through back of luminaire (for secondary dimming controls by others). Cannot be used with other control options.

SiteWise (SW): SiteWise system includes a controller fully integrated in the luminaire that enables the luminaires to communicate with a dimming signal transmitter cabinet located on site using Philips patented central dimming technology. A locally accessible mobile app allows users to access the system and set functionalities such as ON/OFF, dimming levels and scheduling. SiteWise is available with motion response options in order to bring the light back to 100% when motion is detected. Cannot be used with other control options or photocell options. Additional functionalities are available such as communication with indoor lighting and connection to BMS systems. Complete information on the control system can be found on the SiteWise website at philips.com/sitewise.

Field Adjustable Wattage Selector (FAWS): Luminaire equipped with the ability to manually adjust the wattage in the field to reduce total luminaire lumen output and light levels. Comes pre-set to the highest position at the lumen output selected. Use chart below to estimate reduction in lumen output desired. Cannot be used with other control options or motion response.

FAWS Position	Percent of Typical Lumen Output
1	25%
2	50%
3	55%
4	65%
5	75%
6	80%
7	85%
8	90%
9	95%
10	100%

Note: Typical value accuracy +/- 5%

Automatic Profile Dimming (CS/CM/CE/CA): Standard dimming profile of 30% or 50% provide flexibility towards energy savings goals while optimizing light levels during specific dark hours. When used in combination with not programmed motion response it overrides the controller's schedule when motion is detected. After 5 minutes with no motion, it will return to the automatic dimming profile schedule. Automatic dimming profile scheduled with the following settings:

- **CS50/CS30:** Security for 7 hours night duration (Ex., 11 PM - 6 AM)
- **CM50/CM30:** Median for 8 hours night duration (Ex., 10 PM - 6 AM)
- **CE50/CE30:** Economy for 9 hours night duration (Ex., 9 PM - 6 AM)
- **CA50/CA30:** for all night (during all dark hours)

Cannot be used with other control options. .

Motion response options

Bi-Level Infrared Motion Response (BL-IMRI): Motion Response module is mounted integral to luminaire factory pre-programmed to 50% dimming when not ordered with other control options. P50-IMRI is set/operates in the following fashion: The motion sensor is set to a constant 50%. When motion is detected by the PIR sensor, the luminaire returns to full power/light output. Dimming on low is factory set to 50% with 5 minutes default in "full power" prior to dimming back to low. When no motion is detected for 5 minutes, the motion response system reduces the wattage by 50%, to 50% of the normal constant wattage reducing the light level. Other dimming settings can be provided if different dimming levels are required. This can also be done with FSIR-100 Wireless Remote Programming Tool (contact Technical Support for details).

Infrared Motion Response with Other Controls: When used in combination with other controls (Automatic Dimming Profile and SiteWise), motion response device will simply override controller's schedule with the added benefits of a combined dimming profile and sensor detection. In this configuration, the motion response device cannot be re-programmed with FSIR-100 Wireless Remote Programming Tool. The profile can only be re-programmed via the controller.

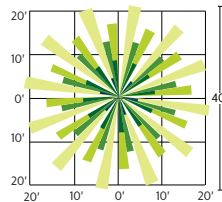
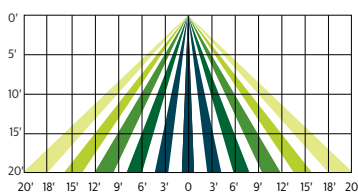
ECF-S EcoForm wildlife

Site & Area

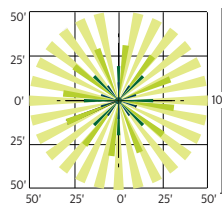
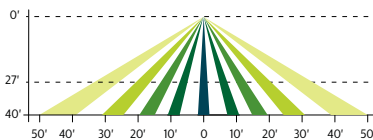
Specifications (continued)

Infrared Motion Response Lenses (IMRI3/IMRI7): Infrared Motion Response Integral module is available with two different sensor lens types to accommodate various mounting heights and occupancy detection ranges. Lens #3 (IMRI3) is designed for mounting heights up to 20' with a 40' diameter coverage area. Lens #7 is designed for higher mounting heights up to 40' with larger coverage areas up to 100' diameter coverage area. See charts for approximate detection patterns:

IMRI3 Luminaire or remote mount controller with #3 lens



IMRI7 Luminaire or remote mount controller with #7 lens



Electrical

Twist-Lock Receptacle (TLRD5/TLRD7/TLRPC): Twist Lock Receptacle with 5 pins enabling dimming or with 7 pins with additional functionality (by others) can be used with a twist-lock photoelectric cell or a shorting cap. Dimming Receptacle Type B (5-pin) and Type D-24 (7-pin) in accordance to ANSI C136.41. Can be used with Philips or third-party control system. Receptacle located on top of luminaire housing. When specifying receptacle with twistlock photoelectric cell, voltage must be specified.

Driver: Driver efficiency (>90% standard). 120-480V available (restrictions apply). Open/short circuit protection. Optional 0-10V dimming to 10% power. RoHS compliant.

Button Photocontrol (PCB): Button style design for internal luminaires mounting applications. The photocontrol is constructed of a high impact UV stabilized polycarbonate housing. Rated voltage of 120V or 208-277V with a load rating of 1000 VA. The photocell will turn on with 1-4Fc of ambient light.

Surge protection (SP1/SP2): Each luminaire is provided as standard with surge protection device tested in accordance with ANSI/IEEE C62.45 per ANSI/IEEE C62.41.2 Scenario I Category C High Exposure 10kV/10kA waveforms for Line-Ground, Line-Neutral and Neutral-Ground, and in accordance with DOE MSSLC Model Specification for LED Roadway Luminaires Appendix D Electrical Immunity High test level 10kV/10kA. Optional twice the IEEE C62.41.2-2002 C-High recommendation (22KA) is available for additional protection.

Retrofit Arm Mount

EcoForm features an innovative retrofit arm kit. When specified with the retrofit arm (RAM) option, EcoForm seamlessly simplifies site conversions to LED by eliminating the need for additional pole drilling on most existing poles. RAM will be boxed separately.

Listings

UL/cUL listed to the UL 1598 standard, suitable for Wet Locations. Suitable for use in ambients from -40° to 40°C (-40° to 104°F). The quality systems of this facility have been registered by UL to the ISO 9001 series standards. Florida Wildlife Certified.

Finish

Each standard color luminaire receives a fade and abrasion resistant, electrostatically applied, thermally cured, triglycidal isocyanurate (TGIC) textured polyester powdercoat finish. Standard colors include bronze (BZ), black (BK), white (WH), dark gray (DGY), and medium gray (MGY). Consult factory for specs on optional or custom colors.

Warranty

EcoForm luminaires feature a 5 year limited warranty. See signify.com/warranties for complete details and exclusions.





Mitigated Negative Declaration

Sonoma County Permit and Resource Management Department (Permit Sonoma)

2550 Ventura Avenue, Santa Rosa, CA 95403
(707) 565-1900 FAX (707) 565-1103

Publication Date: February 24, 2021
Public Review Period: Feb. 24 – March 26, 2021
State Clearinghouse Number: 2021020421
Permit Sonoma File Number: PLP19-0045 (DRH19-0007
and UPE19-0081)
Prepared by: Katrina Braehmer
Phone: (707) 565-1903

Pursuant to Section 15071 of the State CEQA Guidelines, this proposed Mitigated Negative Declaration and the attached Initial Study, including the identified mitigation measures and monitoring program, constitute the environmental review conducted by the County of Sonoma as lead agency for the proposed project described below:

Project Name: NeilMed Warehouse
Project Applicant/Operator: Ken DiLillo
Project Location/Address: 685 Aviation Boulevard, Santa Rosa
APN: 059-340-056
General Plan Land Use Designation: LI
Zoning Designation: MP 1 AC AVG, VOH
Decision Making Body: Sonoma County Design Review Committee
Appeal Body: Sonoma County Planning Commission
Project Description: See below

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a “Potentially Significant Impact” or “Less than Significant with Mitigation” as indicated in the attached Initial Study and in the summary table below.

Table 1. Summary of Topic Areas

Topic Area	Abbreviation	Yes	No
Aesthetics	VIS		X
Agricultural & Forest Resources	AG		X
Air Quality	AIR	X	
Biological Resources	BIO	X	
Cultural Resources	CUL	X	
Energy	ENE		X
Geology and Soils	GEO		X

Greenhouse Gas Emission	GHG	X	
Hazards and Hazardous Materials	HAZ		X
Hydrology and Water Quality	HYDRO		X
Land Use and Planning	LU		X
Mineral Resources	MIN		X
Noise	NOISE	X	
Population and Housing	POP		X
Public Services	PS		X
Recreation	REC		X
Transportation	TRAF		X
Tribal Cultural Resources	TCR		X
Utility and Service Systems	UTL		X
Wildfire	WILD		X
Mandatory Findings of Significance			X

RESPONSIBLE AND TRUSTEE AGENCIES

The following lists other public agencies whose approval is required for the project, or who have jurisdiction over resources potentially affected by the project.

Table 2. Responsible and Trustee Agencies

Table 2. Agency	Activity	Authorization
Sonoma County Permit and Resource Management Department (Permit Sonoma)	Requires that grading, septic and building permits be obtained for development of this site	
Bay Area Air Quality Management District (BAAQMD)	Stationary air emissions	BAAQMD Rules and Regulations (Regulation 2, Rule 1 – General Requirements; Regulation 2, Rule 2 – New Source Review; Regulation 9 – Rule 8 – NOx and CO from Stationary Internal Combustion Engines; and other BAAQMD administered Statewide Air Toxics Control Measures (ATCM) for stationary diesel engines
California Department of Fish and Wildlife	Impacts to species or habitat	California Endangered Species Act; Sections of the California Fish and Game Code related to Fully Protected Species, nongame mammals, nesting birds, and California Species of Special Concern
U. S. Fish and Wildlife Service (FWS) and or National Marine Fisheries Service (NMFS)	Impacts to species or habitat	Endangered Species Act

ENVIRONMENTAL FINDING:

Based on the evaluation in the attached Initial Study, I find that the project described above will not have a significant adverse impact on the environment, provided that the mitigation measures identified in the Initial Study are incorporated as conditions of approval for the project, and a Mitigated Negative Declaration has been prepared. The applicant has agreed in writing to incorporate identified mitigation measure into the project plans.



Prepared by: Katrina Braehmer

Date: February 24, 2021



Expanded Initial Study

Sonoma County Permit and Resource Management Department (Permit Sonoma)

2550 Ventura Avenue, Santa Rosa, CA 95403
(707) 565-1900 FAX (707) 565-1103

I. INTRODUCTION

Ken Di Lillo, on behalf of the property owner Alisha and Natasha Properties, LLC, proposes a new 59,066 square foot warehouse building with 500 square of office space on a 1.53 acre parcel located in the Airport Business Park in north Santa Rosa. A referral letter was sent to the appropriate local, state and federal agencies and interest groups who may wish to comment on the project.

This report is the Initial Study required by the California Environmental Quality Act (CEQA). The report was prepared by Katrina Braehmer, Project Review Planner with the Sonoma County Permit and Resource Management Department (Permit Sonoma), Project Review Division. Information on the project was provided by Ken Di Lillo (applicant), Del Starrett (architect), and Patrick Imbimbo. Technical studies provided by qualified consultants are attached to this Expanded Initial Study to support the conclusions. Other reports, documents, maps and studies referred to in this document are available for review at the Permit and Resource Management Department (Permit Sonoma) or at <https://share.sonoma-county.org/link/mnvf9w9r4J4/>.

Please contact Katrina Braehmer, Project Planner, at (707) 565-1903 for more information.

II. PROJECT DESCRIPTION

Ken Di Lillo proposes to develop a new two-story, 59,066 square foot warehouse for dry-goods, with two truck loading docks on a 1.53-acre parcel. Other site improvements include paved parking and circulation areas, landscaping, and site lighting. The purpose of the project is to supplement existing warehouse buildings on adjacent properties under the same ownership. Water supply will be provided by the Town of Windsor and the warehouse will connect to the Airport/Larkfield/Wikiup Sanitation District (Sonoma Water) for domestic wastewater service. The warehouse will not contain drains and there will be no industrial wastewater. Access is secured from Aviation Boulevard by easement through the properties to the south and the east of the site, although truck ingress/egress will be from existing driveway that serves 601 Aviation Boulevard. Twenty employees are anticipated for warehouse operations, which will occur on weekdays from 5:00 am to 12:00 am.

The 50-foot tall warehouse is proposed to have precast concrete panels for the exterior walls, with the east elevation comprising the central entry and second-story observation balcony. The two stories are served by two stairwells, a freight elevator, and a passenger elevator. The two recessed loading docks will be on the north side of the building, and three other surface level loading doors are provided. The final parking configuration will include 43 spaces onsite, with 14 dedicated to the office building south of the site. Two stalls will have electric vehicle charging stations, three will be reserved for clean air or vanpool vehicles, and two will be ADA-compliant. Landscaping will be installed at the western and northern perimeter of the parcel and along the eastern and southern edges of the building.

III. SETTING

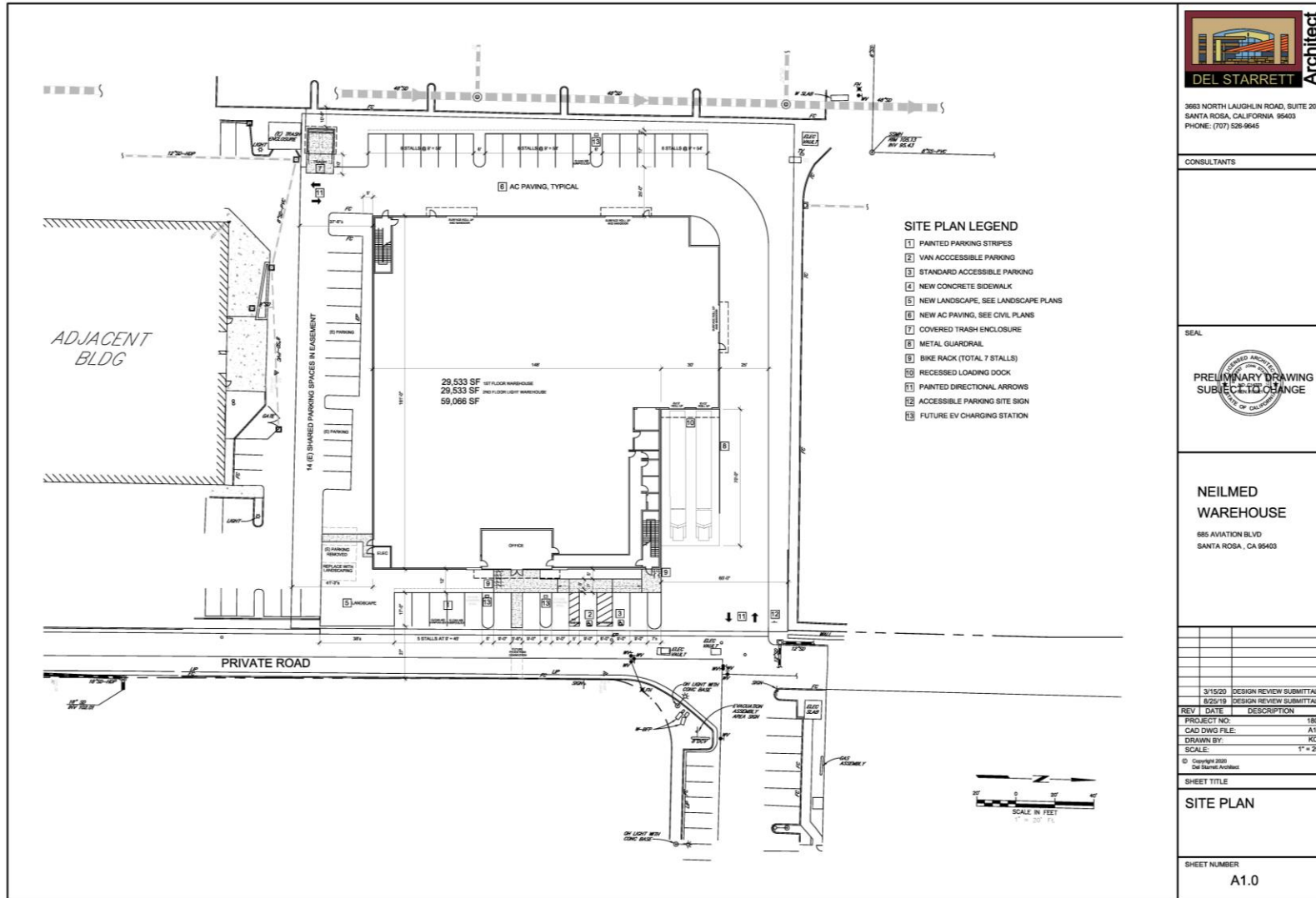
The project site is located in the Airport Business Park in north Santa Rosa, approximately 2.75 miles

from the City of Santa Rosa limits and just west of the Charles M. Schulz Sonoma County Airport. The Airport Business Park is zoned MP (Industrial Park) and surrounding land uses are a mix of light manufacturing and warehousing, and commercial offices. The General Plan Land Use Designation of the parcel and surrounding area is Limited Industrial. Skylane Boulevard and Airport Boulevard have Class II bikeways. The business park is subject to the Airport Industrial Specific Plan.

The site is generally flat at approximately 110 feet above sea level. Soils at the site are mapped as Huichica loam and vegetation is primarily disturbed California annual grassland. Discernible species on the site on November 12, 2019 were harding grass (*Phalaris aquatica*), riggut grass (*Bromus diandrus*), slender wild oat (*Avena barbata*), and soft chess (*Bromus hordeaceus*). Common non-native forbs include bindweed (*Convolvulus arvensis*), bristly ox-tongue (*Helminthotheca echioides*), English plantain (*Plantago lanceolata*), radish (*Raphanus sativus*), rough cat's-ear (*Hypochaeris radicata*), and vetch (*Vicia sativa*). There were patches of Himalayan blackberry (*Rubus armeniacus*) along the perimeter of the site as well as emergent coast live oak trees (*Quercus agrifolia*) and ornamentals including Chinese pistache (*Pistacia chinensis*) and thorny olive (*Elaeagnus pungens*). In the northern portion of the project site a depression was observed with some hydrophytic vegetation, but the sample point did not meet any of the wetland indicators. Further discussion of wetlands can be found in the biological resources section. The site is located within the Santa Rosa Plain Conservation Area.

Figure 2 shows the project vicinity and Figure 3 provides an aerial view of the project and surrounding area.

Figure 1. Site Plan



DEL STARRETT Architect

3683 NORTH LAUGHLIN ROAD, SUITE 207
 SANTA ROSA, CALIFORNIA 95403
 PHONE: (707) 526-9645

CONSULTANTS

SEAL

**PRELIMINARY DRAWING
 SUBJECT TO CHANGE**

**NEILMED
 WAREHOUSE**

685 AVIATION BLVD
 SANTA ROSA, CA 95403

REV	DATE	DESCRIPTION
PROJECT NO:	1807	
CAD DWG FILE:	A10	
DRAWN BY:	KCT	
SCALE:	1" = 20'	

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 Del Starrett Architect

SHEET TITLE

SITE PLAN

SHEET NUMBER

A1.0

Figure 2. Project Vicinity

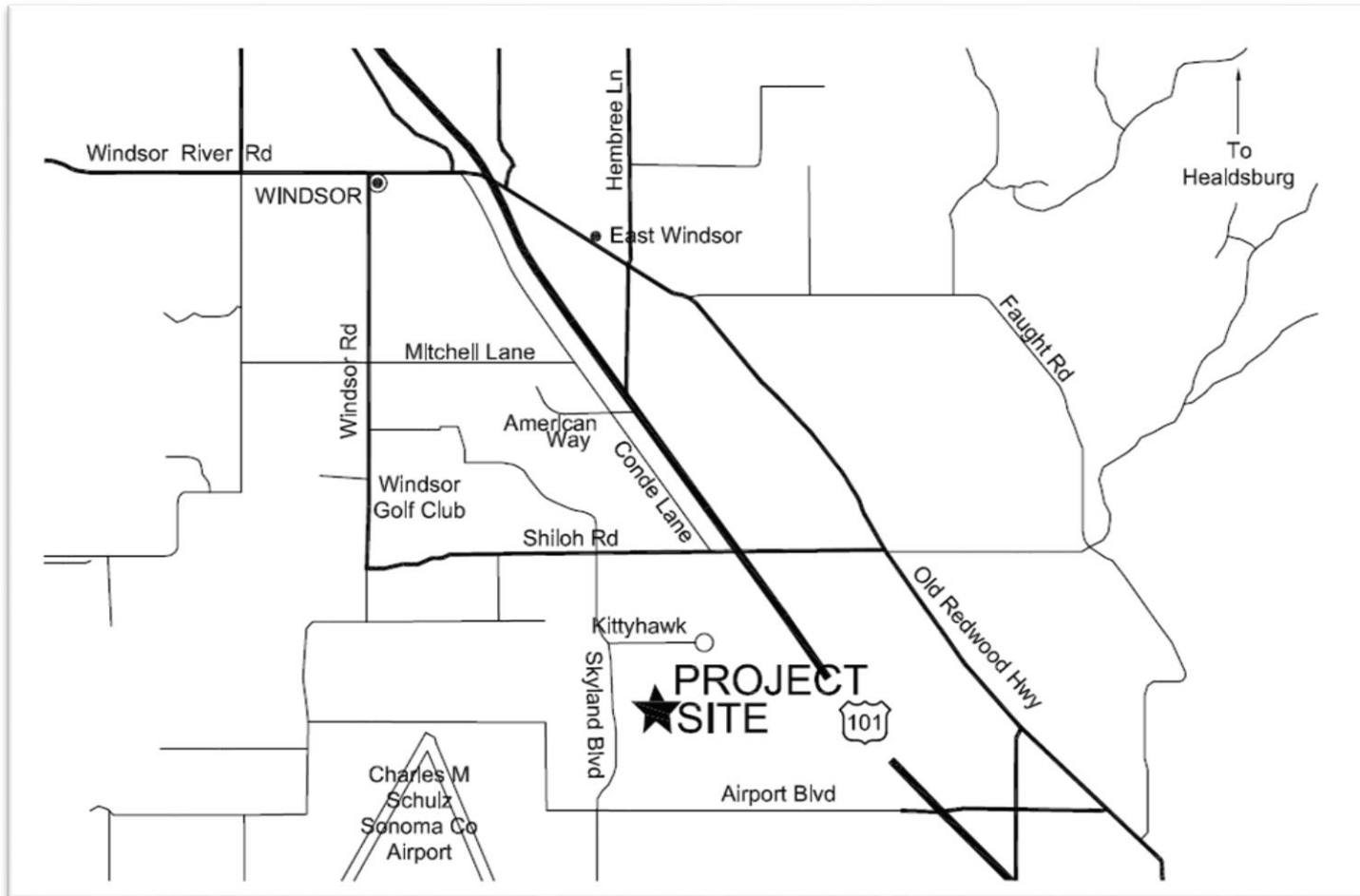


Figure 3. Aerial View



IV. ISSUES RAISED BY THE PUBLIC OR AGENCIES

Agency Referral

A referral packet was drafted and circulated to inform and solicit comments from selected relevant local, state and federal agencies; and to special interest groups that were anticipated to take interest in the project. The Northwest Information Center recommended a cultural resources study, which was subsequently prepared by Tom Origer & Associates. The California Department of Fish and Wildlife indicated that the site should be evaluated for wetlands and endangered plant species. Sol Ecology prepared a biological resources assessment and the results are discussed in the biological resources section of this document.

Tribal Consultation under AB 52

Referrals were sent to the following Tribes on October 21, 2019:

Cloverdale Rancheria of Pomo Indians
Dry Creek Rancheria Band of Pomo Indians
Torres Martinez Desert Cahuilla Indians
Mishewal Wappo Tribe of Alexander Valley
Middletown Rancheria Band of Pomo Indians
Lytton Rancheria of California
Kashia Pomos Stewarts Point Rancheria
Federated Indians of Graton Rancheria

Lytton Rancheria of California requested that a cultural resources survey be conducted. The study prepared by Tom Origer & Associates was sent to Lytton Rancheria, and standard discovery conditions of approval were requested.

Public Comments

No public comment has been received to date.

V. EVALUATION OF ENVIRONMENTAL IMPACTS

This section analyzes the potential environmental impacts of this project based on the criteria set forth in the State CEQA Guidelines and the County's implementing ordinances and guidelines. For each item, one of four responses is given:

No Impact: The project would not have the impact described. The project may have a beneficial effect, but there is no potential for the project to create or add increment to the impact described.

Less Than Significant Impact: The project would have the impact described, but the impact would not be significant. Mitigation is not required, although the project applicant may choose to modify the project to avoid the impacts.

Potentially Significant Unless Mitigated: The project would have the impact described, and the impact could be significant. One or more mitigation measures have been identified that will reduce the impact to a less than significant level.

Potentially Significant Impact: The project would have the impact described, and the impact could be significant. The impact cannot be reduced to less than significant by incorporating

mitigation measures. An environmental impact report must be prepared for this project.

Each question was answered by evaluating the project as proposed, that is, without considering the effect of any added mitigation measures. The Initial Study includes a discussion of the potential impacts and identifies mitigation measures to substantially reduce those impacts to a level of insignificance where feasible. All references and sources used in this Initial Study are listed in the Reference section at the end of this report and are incorporated herein by reference.

Ken Di Lillo has agreed to accept all mitigation measures listed in this Initial Study as conditions of approval for the proposed project, and to obtain all necessary permits, notify all contractors, agents and employees involved in project implementation and any new owners should the property be transferred to ensure compliance with the mitigation measures.

1. AESTHETICS

Except as provided in Public Resources Code Section 21099, would the project:

a) Have a substantial adverse effect on a scenic vista?

Comment

The project site is not located in an area designated as visually sensitive by the Sonoma County General Plan. It is not located on a scenic hillside, nor would it involve tree removal, grading, or construction that could affect a scenic vista. The project site is located in an existing industrial area and would have no impact on scenic vistas in Sonoma County.

Significance Level:

No Impact

b) Substantially damage scenic resources, including, but not limited to trees, rock outcroppings, and historic buildings within a state scenic highway?

Comment

The project is not located on or visible from a state scenic highway. The two officially designated state scenic highways in Sonoma County are Highway 12 and Highway 116. The site is not visible from either of these highways, therefore the project would not result in any impacts to scenic resources associated with a state scenic highway.

Significance Level

No Impact

c) In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?

Comment

The project is in an existing urbanized industrial and commercial area adjacent to other industrial uses. Using the County's Visual Assessment Guidelines¹, the project site is characterized as having

¹ "Visual Assessment Guidelines," Permit Sonoma, January 2019, <https://sonomacounty.ca.gov/PRMD/Regulations/Environmental-Review-Guidelines/Visual-Assessment->

Low visual sensitivity because it is within an urban land use designation, has no historic character, and no significant natural vegetation of aesthetic value to the surrounding community. The project's visual dominance can be categorized as Subordinate, a category applied when proposed project elements generally repeat the form, line, color, texture, and night lighting of surrounding elements; are minimally visible from public views; and do not attract attention separate from existing uses at the project site. The warehouse will blend with other development in the surrounding area. Utilizing the Visual Assessment Guidelines' matrix, the project's visual impact will be less than significant.

Table 1. Thresholds of Significance for Visual Impact Analysis, PRMD Visual Assessment Guidelines

Sensitivity	Visual Dominance			
	<i>Dominant</i>	<i>Co-Dominant</i>	<i>Subordinate</i>	<i>Inevident</i>
<i>Maximum</i>	Significant	Significant	Significant	Less than significant
<i>High</i>	Significant	Significant	Less than significant	Less than significant
<i>Moderate</i>	Significant	Less than significant	Less than significant	Less than significant
<i>Low</i>	Less than significant	Less than significant	Less than significant	Less than significant

Significance Level
 Less than Significant

- d) **Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?**

Comment

The new buildings would introduce new sources of light and glare. However, proposed lighting would not be substantially greater than existing lighting in the business park, and would be located away from public areas. The project is located down an existing driveway, removed from Aviation Boulevard, therefore, no new lighting is proposed on public street frontage.

Because an exterior lighting plan will be reviewed and approved by Design Review Committee prior to issuance of building permits to ensure compliance with County standards and compatibility with the area, this impact will be less than significant.

Significance Level
 Less than Significant

2. AGRICULTURE AND FOREST RESOURCES

In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board.

Would the project:

- a) **Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?**

Comment

The project parcel is mapped as Urban and Built-Up Land and Farmland of Local Importance. Therefore, no Prime Farmland, Unique Farmland, or Farmland of Statewide Importance would be converted to a non-agricultural use.

Significance Level

No Impact

- b) **Conflict with existing zoning for agricultural use, or Williamson Act Contract?**

Comment

The parcel is zoned MP (Industrial Park), which allows warehouse uses, and is not subject to a Williamson Act Land Conservation Contract. The project will not conflict with zoning for agricultural use or lands under a Land Conservation contract.

Significance Level

No Impact

- c) **Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code Section 4526) or timberland zoned Timberland Production (as defined by Government Code Section 51104(g))?**

Comment

The project site is not in a Timberland Production zoning district, and no forested land or commercial timberland is present. Therefore, the proposed project would not conflict with or cause a rezoning of any forest land or timberland zoned Timberland Production.

Significance Level

No Impact

- d) **Result in the loss of forest land or conversion of forest land to non-forest use?**

Comment

The project would not be located on land utilized or zoned for forest land, timberland, or timber production. Therefore, the project would not result in the loss or conversion of forest land.

Significance Level

No Impact

- e) **Involve other changes in the existing environment which, due to their location or nature, could result in conversion of farmland, to non-agricultural use or conversion of forest land to non-forest use?**

Comment

The project does not involve other changes in the environment that could result in conversion of farmland to non-agricultural use or forest land to non-forest use. The property, itself, is not agriculturally zoned. The proposed warehouse project does not include residential development that might result in a nuisance conflict with nearby agricultural uses.

Significance Level

No Impact

3. AIR QUALITY

The methodologies and assumptions used in preparation of this section follow the CEQA Guidelines developed by the Bay Area Air Quality Management District (BAAQMD), as revised in May 2017². Information on existing air quality conditions, federal and state ambient air quality standards, and pollutants of concern was obtained from the U.S. Environmental Protection Agency (U.S. EPA), California Air Resources Board (CARB), and BAAQMD.

Would the project:

- a) **Conflict with or obstruct implementation of the applicable air quality plan?**

Comment

The project is within the jurisdiction of the Bay Area Air Quality Management District (BAAQMD), which is currently designated as a nonattainment area for State and federal ozone standards, the State PM₁₀ standard, and State and federal PM_{2.5} standards. The District has adopted an Ozone Attainment Plan and a Clean Air Plan in compliance with federal and State Clean Air Acts. These plans include measures to achieve compliance with both ozone standards. The plans deal primarily with emissions of ozone precursors (nitrogen oxides [NO_x] and volatile organic compounds, also referred to as Reactive Organic Gases [ROG]). The BAAQMD provides screening criteria in its report, California Environmental Quality Act Air Quality Guidelines (May 2017). The “general light industry” category has a screening size of 541,000 square feet (or 1,249 employees) for operational criteria pollutants and a screening size of 259,000 square feet (or 540 employees) for construction-generated pollutants. Based on these screening criteria, the proposed project would be much smaller in scale and would be well below the emission thresholds for PM₁₀, PM_{2.5}, and ozone precursors, and therefore, would not conflict with or obstruct the implementation of applicable air quality plans.

Significance Level

Less than Significant

- b) **Violate any air quality standard or contribute substantially to an existing or projected air quality violation?**

² Bay Area Air Quality Management District, “California Environmental Quality Act, Air Quality Guidelines,” May 2017, https://www.baaqmd.gov/~media/files/planning-and-research/ceqa/ceqa_guidelines_may2017-pdf.pdf?la=en

Comment

State and Federal standards have been established for the “criteria pollutants”: ozone, carbon monoxide, nitrogen dioxide, sulfur dioxide and particulates (PM10 and PM2.5). The pollutants NOx (nitrogen oxides) and reactive organic gases (ROG) form ozone in the atmosphere in the presence of sunlight. The principal source of ozone precursors is vehicle emissions, although stationary internal combustion engines are also considered a source. Following use of the screening criteria for ROG and NOx, found in the BAAQMD Air Quality Guidelines (Table 3-1), a detailed air quality study is not required, and emissions of criteria pollutants from the project would be less than significant.

Significance Level

Less than Significant

- c) **Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable Federal or State ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?**

Comment

The project will not have a cumulative effect on ozone because it will not generate substantial traffic which would result in substantial emissions of ozone precursors (ROG and NOx).

The project will have no long-term effect on PM_{2.5} and PM₁₀, because all surfaces will be paved, gravel, landscaped or otherwise treated to stabilize bare soils, and dust generation will be insignificant. However, there could be a significant short-term emission of dust (which would include PM_{2.5} and PM₁₀) during construction. These emissions could be significant at the project level, and could also contribute to a cumulative impact. This impact would be reduced to less than significant by including dust control measures as described in the following mitigation measure.

Significance Level

Less than Significant with Mitigation Incorporated

Mitigation

Mitigation Measure AIR-1: The following dust and air quality control measures shall be included in the project:

- a. Water or alternative dust control method shall be sprayed to control dust on construction areas, soil stockpiles, and staging areas during construction as directed by the County.
- b. Trucks hauling soil, sand, and other loose materials over public roads shall cover the loads, or shall keep the loads at least two feet below the level of the sides of the container, or shall wet the load sufficiently to prevent dust emissions.
- c. Vehicle speeds on unpaved areas shall be limited to 15 miles per hour.
- d. Final surfacing (i.e., pavement or concrete, gravel, landscaping) shall be completed as soon as possible after earthwork is finished, unless seeding or soil binders are used.
- e. Idling time of diesel-powered construction equipment shall be limited to five minutes. Signs shall be posted reminding workers of this idling restriction at all access points and equipment staging areas during construction of the proposed project.
- f. All construction equipment shall be maintained and properly tuned in accordance with manufacturer’s specifications and shall have a CARB-certified visible emissions evaluator check equipment prior to use at the site.
- g. Trackout shall not be allowed at any active exit from the project site onto an adjacent

paved public roadway or shoulder of a paved public roadway that exceeds cumulative 25 linear feet and creates fugitive dust visible emissions without cleaning up such trackout within 4 hours of when the Construction Coordinator identifies such excessive trackout, and shall not allow more than 1 quart of trackout to remain on the adjacent paved public roadway or the paved shoulder of the paved public roadway at the end of any workday.

- h. Visible emissions of fugitive dust shall not be allowed during cleanup of any trackout that exceeds 20 percent opacity as determined by the Environmental Protection Agency in *Method 203B - Opacity Determination for Time-Exception Regulations* (August 2017).

Trackout is defined by BAAQMD in *Regulation 6, Rule 6: Prohibition of Trackout* (August 2018) as any sand, soil, dirt, bulk materials or other solid particles from a site that adhere to or agglomerate on the exterior surfaces of vehicles (including tires), and subsequently fall or are dislodged onto a paved public roadway or the paved shoulder of a paved public roadway on the path that vehicles follow at any exit and extending 50 feet out onto the paved public roadway beyond the boundary of the site. Material that has collected on the roadway from erosion is not trackout.

Monitoring AIR-1: Permit Sonoma staff shall verify that the AIR-1 measures are included on all site alteration, grading, building or improvement plans prior to issuance of grading or building permits.

d) Expose sensitive receptors to substantial pollutant concentrations?

Comment

Sensitive receptors are facilities that house or attract children, the elderly, people with illnesses, or others who are especially sensitive to the effects of air pollutants. Hospitals, schools, convalescent facilities, and residential areas are examples of sensitive receptors. Localized impacts to sensitive receptors generally occur when sources of air pollutants and sensitive receptors are located near one another. The project site is residentially zoned with limited agricultural uses and abuts other residential and agricultural parcels. The project would not expose these sensitive receptors to significant concentrations of pollutants because of the analysis above in 3(b) and 3(c). The proposed project would not create an incompatible situation as neither the residential use of the project site nor the neighboring uses involve stationary or point sources of air pollutants which generate substantial pollutant concentrations. Although there will be no long term increase in emissions, during construction of future build-out there could be significant short term dust emissions that would affect nearby residents. Dust emissions can be reduced to less than significant by Mitigation Measure AIR-1.

Significance Level

Less than Significant with Mitigation Incorporated

Mitigation

See Mitigation Measure AIR-1.

e) Create objectionable odors affecting a substantial number of people?

Comment

The project is not an odor-generating use generally, and most operations will occur inside of the warehouse with the exception of loading and unloading at the dock. Construction equipment may generate odors during project construction; however, construction activities would be short-term, intermittent, and would cease upon completion of project construction. In addition, implementation of Mitigation Measure AIR-1 would reduce construction vehicle emissions which could contribute to odor

and would not affect a substantial number of people. Therefore, the construction-related odor impact would be less than significant.

Significance Level

Less than Significant with Mitigation Incorporated

Mitigation

See Mitigation Measure AIR-1.

4. BIOLOGICAL RESOURCES

Regulatory Framework

The following discussion identifies federal, state and local environmental regulations that serve to protect sensitive biological resources relevant to the California Environmental Quality Act (CEQA) review process.

FEDERAL

Federal Endangered Species Act (FESA)

FESA establishes a broad public and federal interest in identifying, protecting, and providing for the recovery of threatened or endangered species. The Secretary of Interior and the Secretary of Commerce are designated in FESA as responsible for identifying endangered and threatened species and their critical habitat, carrying out programs for the conservation of these species, and rendering opinions regarding the impact of proposed federal actions on listed species. The USFWS and the National Oceanic and Atmospheric Administration's National Marine Fisheries Service (NOAA Fisheries) are charged with implementing and enforcing the FESA. USFWS has authority over terrestrial and continental aquatic species, and NOAA Fisheries has authority over species that spend all or part of their life cycle at sea, such as salmonids.

Section 9 of FESA prohibits the unlawful "take" of any listed fish or wildlife species. Take, as defined by FESA, means "to harass, harm, pursue, hunt, shoot, wound, kill, trap, capture, or collect, or to attempt to engage in any such action." USFWS's regulations define harm to mean "an act which actually kills or injures wildlife." Such an act "may include "significant habitat modification or degradation where it actually kills or injures wildlife by significantly impairing essential behavioral patterns, including breeding, feeding or sheltering" (50 CFR § 17.3). Take can be permitted under FESA pursuant to sections 7 and 10.

Section 7 provides a process for take permits for federal projects or projects subject to a federal permit, and Section 10 provides a process for incidental take permits for projects without a federal nexus. FESA does not extend the take prohibition to federally listed plants on private land, other than prohibiting the removal, damage, or destruction of such species in violation of state law.

Critical Habitat

Critical habitat is a term defined in the ESA as a specific geographic area that contains features essential for the conservation of a threatened or endangered species and that may require special management and protection. The ESA requires federal agencies to consult with the USFWS to conserve listed species on their lands and to ensure that any activities or projects they fund, authorize, or carry out will not jeopardize the survival of a threatened or endangered species. In consultation for those species with critical habitat, federal agencies must also ensure that their activities or projects do not adversely modify critical habitat to the point that it will no longer aid in the species' recovery. In many cases, this level of protection is similar to that already provided to species by the ESA jeopardy standard. However, areas that are currently unoccupied by the species but which are needed for the species' recovery are protected

by the prohibition against adverse modification of critical habitat.

Essential Fish Habitat

Essential Fish Habitat (EFH) is regulated through the NMFS, a division of the National Oceanic and Atmospheric Administration (NOAA). Protection of Essential Fish Habitat is mandated through changes implemented in 1996 to the Magnuson-Stevens Fishery Conservation and Management Act (Magnuson-Stevens Act) to protect the loss of habitat necessary to maintain sustainable fisheries in the United States. The Magnuson-Stevens Act defines Essential Fish Habitat as "those waters and substrate necessary to fish for spawning, breeding, feeding, or growth to maturity" [16 USC 1802(10)]. NMFS further defines essential fish habitat as areas that "contain habitat essential to the long-term survival and health of our nation's fisheries" Essential Fish Habitat can include the water column, certain bottom types such as sandy or rocky bottoms, vegetation such as eelgrass or kelp, or structurally complex coral or oyster reefs. Under regulatory guidelines issued by NMFS, any federal agency that authorizes, funds, or undertakes action that may affect EFH is required to consult with NMFS (50 CFR 600.920).

The Migratory Bird Treaty Act of 1918 (MBTA)

The U.S. MBTA (16 USC §§ 703 et seq., Title 50 Code of Federal Regulations [CFR] Part 10) states it is "unlawful at any time, by any means or in any manner, to pursue, hunt, take, capture, kill; attempt to take, capture or kill; possess, offer for sale, sell, offer to barter, barter, offer to purchase, purchase, deliver for shipment, ship, export, import, cause to be shipped, exported, or imported, deliver for transportation, transport or cause to be transported, carry or cause to be carried, or receive for shipment, transportation, carriage, or export any migratory bird, any part, nest, or egg of any such bird, or any product, whether or not manufactured, which consists, or is composed in whole or in part, of any such bird or any part, nest or egg thereof..." In short, under MBTA it is illegal to disturb a nest that is in active use, since this could result in killing a bird, destroying a nest, or destroying an egg. The USFWS enforces MBTA. The MBTA does not protect some birds that are non-native or human-introduced or that belong to families that are not covered by any of the conventions implemented by MBTA. In 2017, the USFWS issued a memorandum stating that the MBTA does not prohibit incidental take; therefore, the MBTA is currently limited to purposeful actions, such as directly and knowingly removing a nest to construct a project, hunting, and poaching.

The Clean Water Act (CWA)

The CWA is the primary federal law regulating water quality. The implementation of the CWA is the responsibility of the U.S. Environmental Protection Agency (EPA). However, the EPA depends on other agencies, such as the individual states and the U.S. Army Corps of Engineers (USACE), to assist in implementing the CWA. The objective of the CWA is to "restore and maintain the chemical, physical, and biological integrity of the Nation's waters." Section 404 and 401 of the CWA apply to activities that would impact waters of the U.S. The USACE enforces Section 404 of the CWA and the California State Water Resources Control Board enforces Section 401.

Section 404.

As part of its mandate under Section 404 of the CWA, the EPA regulates the discharge of dredged or fill material into "waters of the U.S.". "Waters of the U.S. include territorial seas, tidal waters, and non-tidal waters in addition to wetlands and drainages that support wetland vegetation, exhibit ponding or scouring, show obvious signs of channeling, or have discernible banks and high-water marks. Wetlands are defined as those areas "that are inundated or saturated by surface or groundwater at a frequency and duration sufficient to support, and that under normal circumstances do support a prevalence of vegetation typically adapted for life in saturated soil conditions" (33 CFR 328.3(b)). The discharge of dredged or fill material into waters of the U.S. is prohibited under the CWA except when it is in compliance with Section 404 of

the CWA. Enforcement authority for Section 404 was given to the USACE, which it accomplishes under its regulatory branch. The EPA has veto authority over the USACE's administration of the Section 404 program and may override a USACE decision with respect to permitting. Substantial impacts to waters of the U.S. may require an Individual Permit's Projects that only minimally affect waters of the U.S. may meet the conditions of one of the existing Nationwide Permits, provided that such permit's other respective conditions are satisfied. A Water Quality Certification or waiver pursuant to Section 401 of the CWA is required for Section 404 permit actions (see below).

Section 401.

Any applicant for a federal permit to impact waters of the U.S. under Section 404 of the CWA, including Nationwide Permits where pre-construction notification is required, must also provide to the USACE a certification or waiver from the State of California. The "401 Certification" is provided by the State Water Resources Control Board through the local Regional Water Quality Control Board (RWQCB). The RWQCB issues and enforces permits for discharge of treated water, landfills, storm-water runoff, filling of any surface waters or wetlands, dredging, agricultural activities and wastewater recycling. The RWQCB recommends the "401 Certification" application be made at the same time that any applications are provided to other agencies, such as the USACE, USFWS, or NOAA Fisheries. The application is not final until completion of environmental review under the CEQA. The application to the RWQCB is similar to the pre-construction notification that is required by the USACE. It must include a description of the habitat that is being impacted, a description of how the impact is proposed to be minimized and proposed mitigation measures with goals, schedules, and performance standards. Mitigation must include a replacement of functions and values, and replacement of wetland at a minimum ratio of 2:1, or twice as many acres of wetlands provided as are removed. The RWQCB looks for mitigation that is on site and in-kind, with functions and values as good as or better than the water-based habitat that is being removed.

STATE

California Endangered Species Act (CESA)

Provisions of CESA protect state-listed threatened and endangered species. The CDFW is charged with establishing a list of endangered and threatened species. CDFW regulates activities that may result in "take" of individuals (i.e., "hunt, pursue, catch, capture, or kill, or attempt to hunt, pursue, catch, capture, or kill"). Habitat degradation or modification is not expressly included in the definition of "take" under the California Fish and Game Code (CFGC), but CDFW has interpreted "take" to include the killing of a member of a species which is the proximate result of habitat modification.

Fish and Game Code 1600-1602

Sections 1600-1607 of the CFGC require that a Notification of Lake or Streambed Alteration Agreement (LSAA) application be submitted to CDFW for "any activity that may substantially divert or obstruct the natural flow or substantially change the bed, channel, or bank of any river, stream, or lake." CDFW reviews the proposed actions in the application and, if necessary, prepares a LSAA that includes measures to protect affected fish and wildlife resources, including mitigation for impacts to bats and bat habitat.

Nesting Birds

Nesting birds, including raptors, are protected under CFGC Section 3503, which reads, "It is unlawful to take, possess, or needlessly destroy the nest or eggs of any bird, except as otherwise provided by this code or any regulation made pursuant thereto." In addition, under CFGC Section 3503.5, "it is unlawful to take, possess, or destroy any birds in the orders Falconiformes or Strigiformes (birds-of-prey) or to take, possess, or destroy the nest or eggs of any such bird except as otherwise provided by this code or any regulation adopted pursuant thereto". Passerines and non-passerine land birds are further protected

under CFGC 3513. As such, CDFW typically recommends surveys for nesting birds that could potentially be directly (e.g., actual removal of trees/vegetation) or indirectly (e.g., noise disturbance) impacted by project-related activities. Disturbance during the breeding season could result in the incidental loss of fertile eggs or nestlings, or otherwise lead to nest abandonment. Disturbance that causes nest abandonment and/or loss of reproductive effort is considered “take” by CDFW.

Non-Game Mammals

Sections 4150-4155 of the CFGC protects non-game mammals, including bats. Section 4150 states “A mammal occurring naturally in California that is not a game mammal, fully protected mammal, or fur-bearing mammal is a nongame mammal. A non-game mammal may not be taken or possessed except as provided in this code or in accordance with regulations adopted by the commission”. The non-game mammals that may be taken or possessed are primarily those that cause crop or property damage. Bats are classified as a non-game mammal and are protected under the CFGC.

California Fully Protected Species

The classification of “fully protected” was the CDFW’s initial effort to identify and provide additional protection to those animals that were rare or faced possible extinction. Lists were created for fish, amphibians and reptiles, birds, and mammals. Most of the species on these lists have subsequently been listed under CESA and/or FESA. The Fish and Game Code sections (fish at §5515, amphibians and reptiles at §5050, birds at §3503 and §3511, and mammals at §4150 and §4700) dealing with “fully protected” species state that these species “...may not be taken or possessed at any time and no provision of this code or any other law shall be construed to authorize the issuance of permits or licenses to take any fully protected species,” although take may be authorized for necessary scientific research. This language makes the “fully protected” designation the strongest and most restrictive regarding the “take” of these species. In 2003, the code sections dealing with “fully protected” species were amended to allow the CDFW to authorize take resulting from recovery activities for state-listed species.

Species of Special Concern

California Species of Special Concern (CSC) are broadly defined as animals not listed under the FESA or CESA, but which are nonetheless of concern to the CDFW because they are declining at a rate that could result in listing or because they historically occurred in low numbers and known threats to their persistence currently exist. This designation is intended to result in special consideration for these animals by the CDFW, land managers, consulting biologists, and others, and is intended to focus attention on the species to help avert the need for costly listing under FESA and CESA and cumbersome recovery efforts that might ultimately be required. This designation also is intended to stimulate collection of additional information on the biology, distribution, and status of poorly known at-risk species, and focus research and management attention on them. Although these species generally have no special legal status, they are given special consideration under the CEQA during project review.

Porter-Cologne Water Quality Control Act

The intent of the Porter-Cologne Water Quality Control Act (Porter-Cologne) is to protect water quality and the beneficial uses of water, and it applies to both surface and ground water. Under this law, the State Water Resources Control Board develops statewide water quality plans, and the RWQCBs develop basin plans that identify beneficial uses, water quality objectives, and implementation plans. The RWQCBs have the primary responsibility to implement the provisions of both statewide and basin plans. Waters regulated under Porter-Cologne, referred to as “waters of the State,” include isolated waters that are not regulated by the USACE. Projects that require a USACE permit, or fall under other federal jurisdiction, and have the potential to impact waters of the State are required to comply with the terms of the Water Quality Certification Program. If a proposed project does not require a federal license or permit,

any person discharging, or proposing to discharge, waste (e.g., dirt) to waters of the State must file a Report of Waste Discharge and receive either waste discharge requirements (WDRs) or a waiver to WDRs before beginning the discharge.

LOCAL

Sonoma County General Plan

The *Sonoma County General Plan 2020* Land Use Element and Open Space & Resource Conservation Element both contain policies to protect natural resource lands including, but not limited to, watershed, fish and wildlife habitat, biotic areas, and habitat connectivity corridors.

Valley Oak Habitat (VOH) Combining District

The VOH combining district is established to protect and enhance valley oaks and valley oak woodlands and to implement the provisions of Sonoma County General Plan 2020 Resource Conservation Element Section 5.1. Design review approval may be required of projects in the VOH, which would include measures to protect and enhance valley oaks on the project site, such as requiring that valley oaks shall comprise a minimum of fifty percent (50%) of the required landscape trees for the development project.

Riparian Corridor (RC) Combining District

The RC combining district is established to protect biotic resource communities, including critical habitat areas within and along riparian corridors, for their habitat and environmental value, and to implement the provisions of the General Plan Open Space and Resource Conservation and Water Resources Elements. These provisions are intended to protect and enhance riparian corridors and functions along designated streams, balancing the need for agricultural production, urban development, timber and mining operations, and other land uses with the preservation of riparian vegetation, protection of water resources, floodplain management, wildlife habitat and movement, stream shade, fisheries, water quality, channel stability, groundwater recharge, opportunities for recreation, education and aesthetic appreciation and other riparian functions and values.

Sonoma County Tree Protection Ordinance

The Sonoma County Tree Protection Ordinance (Sonoma County Code of Ordinances, Chapter 26, Article 88, Sec. 26-88-010 [m]) establishes policies for protected tree species in Sonoma County. Protected trees are defined (Chapter 26, Article 02, Sec. 26- 02-140) as the following species: big leaf maple (*Acer macrophyllum*), black oak (*Quercus kelloggii*), blue oak (*Quercus douglasii*), coast live oak (*Quercus agrifolia*), interior live oak (*Quercus wislizenii*), madrone (*Arbutus menziesii*), oracle oak (*Quercus morehus*), Oregon oak (*Quercus garryana*), redwood (*Sequoia sempervirens*), valley oak (*Quercus lobata*), California bay (*Umbellularia californica*), and their hybrids.

Would the project:

- a) **Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?**

Regulatory Framework

Special-Status Species

Special-status species include those plant and wildlife species that have been formally listed, are

proposed as endangered or threatened, or are candidates for such listing under the federal Endangered Species Act (ESA) or California Endangered Species Act (CESA). These acts afford protection to both listed and proposed species. In addition, California Department of Fish and Wildlife (CDFW) Species of Special Concern, which are species that face extirpation in California if current population and habitat trends continue, U.S. Fish and Wildlife Service (The Service) Birds of Conservation Concern, and CDFW special-status invertebrates, are all considered special-status species. Although CDFW Species of Special Concern generally have no special legal status, they are given special consideration under the California Environmental Quality Act (CEQA). In addition to regulations for special-status species, most birds in the United States, including non-status species, are protected by the Migratory Bird Treaty Act of 1918. Plant species on California Native Plant Society (CNPS) Inventory of Rare and Endangered Plants with California Rare Plant Ranks (Rank) of 1 and 2 are also considered special-status plant species and must be considered under CEQA. Bat species designated as "High Priority" by the Western Bat Working Group (WBWG) qualify for legal protection under Section 15380(d) of the CEQA Guidelines. Species designated High Priority" are defined as "imperiled or are at high risk of imperilment based on available information on distribution, status, ecology and known threats.

Comment

Dana Riggs, Principal Biologist at Sol Ecology, performed an assessment of biological resources, which included a field survey on November 12, 2019, to evaluate the potential presence of sensitive biological communities, the potential for the site to support special-status plant and wildlife species, and the potential presence of any other sensitive natural resources protected by local, state, or federal laws and regulations. The assessment³, dated December 9, 2019 and updated on July 13, 2020, found:

- No sensitive vegetation communities, such as wetlands, on the site;
- Low potential for three special status plant species to be present on site and impacted by the project, including Bent-flowered fiddleneck (*Amsinckia lunaris*), Congested-headed hayfield tarplant (*Hemizonia congesta ssp. congesta*), and Two-fork clover (*Trifolium amoenum*); and
- Low potential for two special-status wildlife species including California tiger salamander (*Ambystoma californiense*) and burrowing owl (*Athene cunicularia*), and nesting birds generally to be present on the site and impacted by the project.

No special-status plant or wildlife species were observed during the field survey. The above conclusions were substantiated by the field survey, a literature and records review, and a plant survey report by Ted Winfield & Associates dated August 2, 2016⁴. Additionally, a Senior Environmental Specialist from Permit Sonoma's Natural Resources section conducted a site visit in August 2020 with biologists from Sol Ecology to review site conditions. The results of the site visit were summarized by Sol Ecology in a memorandum dated August 27, 2020⁵.

Vegetation onsite is primarily disturbed California non-native annual grassland with discernible species including harding grass (*Phalaris aquatica*), ripgut grass (*Bromus diandrus*), slender wild oat (*Avena barbata*), and soft chess (*Bromus hordeaceus*). Common non-native forbs include bindweed (*Convolvulus arvensis*), bristly ox-tongue (*Helminthotheca echioides*), English plantain (*Plantago lanceolata*), radish (*Raphanus sativus*), rough cat's-ear (*Hypochaeris radicata*), and vetch (*Vicia sativa*). There are patches of Himalayan blackberry (*Rubus armeniacus*) along the perimeter of the

³ Riggs, Dana, "Biological Resources Report, 685 Aviation Boulevard, Santa Rosa, Sonoma County, CA", Sol Ecology, July 13, 2020

⁴ Winfield, Ted, Ph.D, "Results of 2016 Plant Survey at Aviation Blvd. Property (APN 059-340-056)", Ted Winfield & Associates, August 2, 2016

⁵ Riggs, Dana, and Mark Kalnins, "Supplemental Wetland Delineation Study, 685 Aviation Blvd., Santa Rosa (APN 059-340-056)", Sol Ecology, August 27, 2020

site as well as emergent coast live oak trees (*Quercus agrifolia*) and ornamentals including Chinese pistache (*Pistacia chinensis*) and thorny olive (*Elaeagnus pungens*). Soils at the site are mapped as Huichica loam, ponded, 0 to 5 percent slopes and Huichica loam, shallow, ponded, 0 to 5 percent slopes.

Special-status Plant Species

Three special-status plant species were determined to have low potential to occur onsite:

- Bent-flowered fiddleneck (*Amsinckia lunaris*) – CNPS Rank 1B.2
- Two-fork clover (*Trifolium amoenum*) – Endangered (Federal Listing), CNPS Rank 1B.1
- Congested-headed hayfield tarplant (*Hemizonia congesta* ssp. *congesta*) – CNPS Rank 1B.2

It is highly unlikely that other special-status species beyond those listed above would be present because conditions necessary to support habitat for those species are not present on the site (e.g. hydrologic, soil, topographic, or unique PH conditions; or upland forest).

All three species with minor potential for presence are typically found in grassland habitat, though bent-flowered fiddleneck and two-fork clover may also be found in coastal bluffs. Bent-flowered fiddleneck may also occur in woodland habitat. Marginally suitable grassland habitat is present on the site for these species. The potential for presence is low due to lack of habitat and given that they were not found during the April 2016 plant surveys conducted by Ted P. Winfield or the November 2019 survey by Sol Ecology. Although onsite habitat is not highly suitable and the potential for presence is low, mitigation measure BIO-1 would ensure the potential impact is less than significant.

Special-status Wildlife Species

Burrowing owl (Athene cunicularia)

Burrowing owl is a CDFW Species of Special Concern and USFWS Bird of Conservation Concern. The burrowing owl occurs as a year-round resident and winter visitor in much of California's lowlands, inhabiting open areas with sparse or non-existent tree or shrub canopies. Typical habitat is annual or perennial grassland, although human-modified areas such as agricultural lands and airports are also used. This species is dependent on burrowing mammals to provide the burrows that are characteristically used for shelter and nesting, and in northern California is typically found in close association with California ground squirrels.

A single occurrence of burrowing owl from 2017 is documented within one mile to the west of the project site, near the airport. This occurrence was observed to be a winter visitor residing in a rip-rapped culvert. While generally suitable grassland habitat is present on the site, the species is not expected to be present due to the lack of available cover (culverts, debris, suitably sized burrows). Furthermore, feeding and housing of a feral cat colony was observed on the northern portion of site. Feral cats are a primary predator of burrowing owl and also small burrowing mammals that are the prey base and shelter providers for this species. Based on site conditions, there is low potential for this species to be present and impacted by the project.

Nesting Birds

Birds and raptors are protected under the federal Migratory Bird Treaty Act (50 CFR 10.13), and their nest, eggs, and young are also protected under the California Fish and Wildlife Code (§3503, §3503.5, and §3800). In addition, raptors such as the white-tailed kite are "fully protected" under the Fish and Wildlife Code (§3511). Fully protected raptors cannot be taken or possessed at any time. Trees along the perimeter of the project site could provide suitable nesting habit for birds. One tree is proposed for relocation on the site, and one other may be removed then replaced. No impacts to nesting birds or their habitat are anticipated, however, if nesting birds are present in trees at the

project site, the relocation or removal, and construction noise would have the potential to impact these species. Mitigation Measure BIO-2 would reduce the impact to nesting birds to a less than significant level.

California tiger salamander (Ambystoma californiense)

The Sonoma County Distinct Population Segment (DPS) of the California tiger salamander (CTS) is listed as Endangered under the ESA and Threatened under the CESA. CTS is a large terrestrial salamander restricted to grasslands and low-elevation foothill regions in California (generally under 1500 feet) where it uses seasonal aquatic habitats for breeding. This salamander breeds in natural ephemeral pools, or ponds that mimic ephemeral pools (e.g., stock ponds that go dry), and occupy substantial areas surrounding the breeding pool as adults. Larval CTS require at least 10-12 weeks to complete their larval stage, so pools must be inundated for at least this long for them to support successful CTS recruitment. CTS spend most of their life in grasslands surrounding breeding pools, surviving hot, dry summers by living underground in burrows such as those created by ground squirrels, gophers or other mammals. Individuals may also use deep cracks or holes in the ground where the soil atmosphere remains near the water saturation point of breeding pools. During wet periods, CTS may emerge from refugia and feed in the surrounding grasslands.

There are no aquatic features on or in the immediate vicinity of the project site that would provide suitable breeding habitat for CTS. As further discussed in section 4(c), there are no seasonal wetlands onsite. The site contains suitable grassland habitat and very few small mammal burrows that could be utilized by CTS. The nearest documented occurrence is 3.1 miles from the site, which is beyond the documented dispersal range of this species. There is a low possibility of CTS being present on site. Mitigation as required by the 2020 Programmatic Biological Opinion for the Santa Rosa Plain will reduce potential impacts to CTS to a less than significant level. See Mitigation Measure BIO-3.

Significance

Less than Significant with Mitigation Incorporated

Mitigation

Mitigation Measure BIO-1: If initial ground disturbance occurs during the flowering periods for bent-flowered fiddleneck (March-June), two-fork clover (April-June), or congested-headed hayfield tarplant (April-November), a qualified biologist shall conduct a pre-construction survey of the disturbance area prior to construction activities. Surveys shall be either focused or protocol-level surveys and follow methodologies outlined in relevant agency protocols. If special-status plants are observed, their locations shall be mapped and CDFW shall be contacted to determine the appropriate mitigation measure to avoid impacts on the species, including avoidance, creation of buffers, transplantation, or otherwise off-site mitigation.

Monitoring BIO-1: Prior to issuance of any grading permit(s) and through completion of initial site disturbance, the County shall review the results of all pre-construction surveys and any measures recommended by the biologist or CDFW to protect sensitive habitat or species. All measures shall be noted on the final project plans.

Mitigation Measure BIO-2: If initial ground disturbance or tree removal occurs during the breeding season for nesting birds (February 1 through August 31), a qualified biologist shall conduct a breeding bird survey no more than 7 days prior to ground disturbance to determine if any birds are nesting in underground burrows or dens, or in trees on or adjacent to the project site. If active nests are found close enough to the project site to affect breeding success, the biologist shall establish an appropriate exclusion zone around the nest. This exclusion zone may be modified depending on the

species, nest location, and existing visual buffers, but typically would entail a minimum of 500 feet for raptor species and 300 feet for other migratory species. Once all young have become independent of the nest, vegetation removal and grading may take place in the former exclusion zone. If initial ground disturbance is delayed or there is a break in project activities of more than 7 days within the bird-nesting season, then a follow-up nesting bird survey shall be performed to ensure no nests have been established in the interim.

Monitoring BIO-2: Prior to issuance of any grading permit(s) and through completion of initial site disturbance, the County shall review the results of all pre-construction surveys and any measures recommended by the biologist or CDFW to protect sensitive habitat or species. All measures shall be noted on the final project plans.

Mitigation Measure BIO-3: Prior to issuance of building or site development permits, the applicant is required to provide mitigation for impacts to potential CTS habitat, consistent with requirements of the Santa Rosa Plain Conservation Strategy and the 2020 Programmatic Biological Opinion. All CTS mitigation will be provided at an off-site location and will consist of the purchase of CTS credits from an approved mitigation bank. The appropriate mitigation ratio area shall be no less than 0.2:1 unless the applicant is able to obtain a “no effect” determination or similar clearance by the USFWS.

Monitoring BIO-3: Permit Sonoma staff shall withhold issuance of site development permits until verification is received indicating that CTS mitigation has been completed.

- b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, and regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?**

Comment

There are no stream channels, riparian habitat, oak woodland, or other sensitive natural communities onsite. The site is primarily California annual grassland.

Significance Level

No Impact

- c) Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?**

Comment

There are no wetlands on the project site. In November of 2019, Sol Ecology evaluated the site for wetlands. In the northern portion, a depression was observed with some hydrophytic vegetation in the herb stratum. The area was sampled but it did not meet any of the wetland indicators (hydrophytic vegetation, hydric soil, or hydrology)⁶.

Permit Sonoma staff from the Natural Resources division later met with Sol Ecology at the project site to review site conditions. Subsequent to the site inspection with staff, Sol Ecology performed additional plot-based sampling where County staff thought wetlands may be present, such as areas where slight topographic depressions exist or there were discernible changes in vegetation. No positive indicators of wetlands were found⁷. Vegetation communities were found to include

⁶ Riggs, Dana, “Biological Resources Report, 685 Aviation Boulevard, Santa Rosa, Sonoma County, CA”, Sol Ecology, July 13, 2020

⁷ Riggs, Dana, and Mark Kalnins, “Supplemental Wetland Delineation Study, 685 Aviation Blvd., Santa Rosa (APN 059-340-056)”, Sol Ecology, August 27, 2020

predominantly upland species, including *Phalaris aquatica* (FACU), Harding's grass; *Bromus hordeaceus* (FACU), soft brome; *Acmispon americanus* (UPL), deerweed; *Convolvulus arvensis* (NL), bindweed; *Sonchus oleraceus* (UPL), common sow-thistle; and *Medicago polymorpha* (FACU), toothed medic. Soils sampled in all of the plots were predominantly gravelly, mixed soils, with bright colors (10YR 4/4, 10YR 3/2), and no visible redoximorphic features, thus positive hydric soil indicators were determined to not be present.

Sol Ecology also inspected several small wood mulch piles for potential wetland fill. Review of aerial imagery for the property through Google Earth indicates that several medium shrubs (likely *Baccharis pilularis*, coyote brush; or *Rubus armeniacus*, Himalayan blackberry) were removed between February and September of 2009, and the wood mulch piles were likely left behind when the shrubs were removed. Mulch depth ranged from 2-4 inches in depth, and no buried vegetation was evident beneath the mulch. It was also determined that native soils beneath the mulch did not meet hydric soil field indicators, and wetlands were therefore not likely to be present at the time the shrubs were removed and the wood mulch was placed. Lastly, the areas immediately adjacent to the mulch were observed to be dominated by upland plants with no positive indicators of wetland hydrology. It was therefore concluded that the areas where mulch was observed were not likely to be wetlands prior to placement of the mulch.

Significance Level

No Impact

- d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?**

Comment

No wildlife corridors (e.g., stream channels, protected open space) or wildlife nursery sites (e.g., rookeries, barns, communal nesting areas) occur within or adjacent to the project site. The site is surrounded by dense commercial development on three sides. The project would have no impact on wildlife corridors or nursery sites.

Significance Level

No Impact

- e) Conflict with any local policies or ordinances protecting biological resources, such as tree preservation policy or ordinance?**

Comment

No riparian habitat or valley oak woodland is present on the project site and minor tree removal is proposed. One coast live oak will be relocated on the site and one may be removed, and if so, will be replaced by the same species. The project does not conflict with any local regulations protecting biological resources.

Significance Level

No Impact

- f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?**

Comment

The project site is located within the area of the Santa Rosa Plain Conservation Strategy. Mitigation measures BIO-3 discussed above ensures that the project does not conflict with any local, regional,

state, or federal conservation plans.

Significance Level

Less than Significant with Mitigation Incorporated

Mitigation

See Mitigation Measures BIO-3 and associated monitoring

5. CULTURAL RESOURCES

Would the project:

- a) **Cause a substantial adverse change in the significance of a historical resource pursuant to §15064.5?**

Comment

Elena Marella and Thomas Origer of Tom Origer & Associates conducted a cultural resources evaluation of the project site in December of 2019⁸. No structure, object, or other element meeting the definition of a historical resource was found, therefore there will be no impact.

Significance Level

No Impact

- b) **Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?**

Comment

On October 21, 2019, Permit Sonoma staff referred the project application to Native American Tribes within Sonoma County to request consultation under AB-52. No requests for consultation were received. Lytton Rancheria of California requested that a cultural resources study be prepared.

As mentioned in the discussion of section 5(a), Tom Origer & Associates conducted a cultural resources evaluation of the project site. An intensive field survey was completed by a Taylor Alshuth and Elena Marella on December 10, 2019. Approximately two person hours were spent in the field and conditions were cloudy. Surface examination consisted of walking transects spaced 15-20 meters apart and a hoe was used as needed to expose the ground surface. Ground visibility ranged from excellent to poor, with vegetation being the primary hindrance. Natural obsidian pebbles were observed in the study area. In addition to a surface survey, two hand-dug auger borings were excavated. Each auger boring was excavated to a depth of 150 centimeters below the ground surface with a 4-inch diameter barrel auger. Auger Boring #1 had chunks of concrete present at a depth of 50 centimeters, which suggests prior ground disturbance.

A buried site model indicates that there is a high potential for buried resources; however, no archaeological site indicators were found with the auger borings during the field visit. Construction related to the project could uncover such materials. The following mitigation measure will reduce potential impacts to less than significant.

Significance Level

⁸ Marella, Elena, B.A. and Thomas M. Origer, M.A., "Cultural Resources Study of the Property at 685 Aviation Boulevard, Santa Rosa, Sonoma County, California", Tom Origer and Associates, December 20, 2019

Less than Significant with Mitigation Incorporated

Mitigation

Mitigation Measure CUL-1: All building and/or grading permits shall have the following note printed on grading or earthwork plan sheets:

“If paleontological resources or prehistoric, historic or tribal cultural resources are encountered during ground-disturbing work, all work in the immediate vicinity shall be halted and the operator must immediately notify the Permit and Resource Management Department (Permit Sonoma) – Project Review staff of the find. The operator shall be responsible for the cost to have a qualified paleontologist, archaeologist or tribal cultural resource specialist under contract to evaluate the find and make recommendations to protect the resource in a report to Permit Sonoma.

Paleontological resources include fossils of animals, plants or other organisms. Prehistoric resources include humanly modified stone, shell, or bones, hearths, firepits, obsidian and chert flaked-stone tools (e.g., projectile points, knives, choppers), midden (culturally darkened soil containing heat-affected rock, artifacts, animal bone, or shellfish remains), stone milling equipment, such as mortars and pestles, and certain sites features, places, cultural landscapes, sacred places and objects with cultural value to a California Native American tribe. Historic resources include all by-products of human use greater than fifty (50) years of age including, backfilled privies, wells, and refuse pits; concrete, stone, or wood structural elements or foundations; and concentrations of metal, glass, and ceramic refuse.

If human remains are encountered, work in the immediate vicinity shall be halted and the operator shall notify Permit Sonoma and the Sonoma County Coroner immediately. At the same time, the operator shall be responsible for the cost to have a qualified archaeologist under contract to evaluate the discovery. If the human remains are determined to be of Native American origin, the Coroner must notify the Native American Heritage Commission within 24 hours of this identification so that a Most Likely Descendant can be designated and the appropriate measures implemented in compliance with the California Government Code and Public Resources Code.”

Monitoring CUL-1: Building/grading permits shall not be approved for issuance by Permit Sonoma staff until the above notes are printed on the building and grading plans.

c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

Comment

The cultural resources evaluation conducted by professional archaeologists in December 2019 did not discover any unique paleontological or geological feature on the property, although paleontological features may be uncovered during project-related construction. Mitigation Measure CUL-1 will reduce potential impacts to less than significant.

Significance Level

Less than Significant with Mitigation Incorporated

Mitigation

See Mitigation Measure and Monitoring CUL-1.

d) Disturb any human remains, including those interred outside of dedicated cemeteries?

Comment

No burial sites are known in the vicinity of the project, and the project site has already been disturbed by past construction. Mitigation Measure CUL-1 will reduce potential impacts to less than significant.

Significance Level

Less than Significant with Mitigation Incorporated

Mitigation

See Mitigation Measure and Monitoring CUL-1

6. ENERGY

Would the project:

a) Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

Comment

Long-term energy demand would result from operation of the warehouse, which would include activities such as lighting, heating, and cooling. Although implementation of the project could result in a net increase in energy usage, the increase would not be wasteful nor inefficient because of energy-efficient building design required by Title 24 of the California Building Code.

Significant energy use is not anticipated for project construction. The primary energy use would be the burning of fossil fuels associated with construction vehicles. Mitigation Measure AIR-1 limits idling time of diesel-powered construction equipment to less than five minutes, primarily to control dust and air quality impacts, but this measure would also reduce fuel consumption.

Significance Level

Less than Significant

b) Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

Comment

The County of Sonoma has not adopted a local renewable energy plan; however, the General Plan includes a variety of policies intended to encourage development of renewable energy systems, while protecting sensitive resources and ensuring neighborhood compatibility. Although renewable energy is encouraged, there is no requirement to develop renewable energy sources for industrial development projects, outside of meeting Title 24 requirements discussed above. Additionally, the project is not located in an identified area designated for renewable energy productions nor would the project interfere with the installation of any renewable energy systems. Therefore, the project would not conflict with or obstruct with applicable State and local plans for promoting use of renewable energy and energy efficiency.

Significance Level

Less than Significant

7. GEOLOGY AND SOILS

Would the project:

a) Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:

- i. **Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.**

Existing geologic conditions that could affect new development are considered in this analysis. Impacts of the environment on the project are analyzed as a matter of County policy and not because such analysis is required by CEQA.

Comment

The project site is not within a fault hazard zone as delineated by the Alquist-Priolo fault maps⁹.

Significance Level

No Impact

- ii. **Strong seismic ground shaking?**

Comment

All of Sonoma County is subject to seismic shaking that would result from earthquakes along the San Andreas, Healdsburg-Rodgers Creek, and other faults. By applying geotechnical evaluation techniques and appropriate engineering practices, potential injury and damage from seismic activity can be diminished, thereby exposing fewer people and less property to the effects of a major damaging earthquake. The design and construction of new structures are subject to engineering standards of the California Building Code (CBC), which take into account soil properties, seismic shaking and foundation type. Standard conditions of approval require that building permits be obtained for all construction and that the project meet all standard seismic and soil test/compaction requirements.

Grading permits are required for all project related construction prior to commencement of ground disturbance and therefore, any required earthwork, grading, trenching, backfilling or compaction operations will be done in accordance with the County Subdivision Ordinance (Chapter 25, Sonoma County Code) and erosion control provisions of the Drainage and Storm Water Management Ordinance (Chapter 11, Sonoma County Code and Building Ordinance (Chapter 7, Sonoma County Code).

All project related construction activities are required to comply with the California Building Code regulations for seismic safety (i.e., reinforcing perimeter and/or load bearing walls, bracing parapets, etc.) as part of the permitting process. Construction plans shall be subject to review and approval of Permit Sonoma prior to the issuance of a building permit. All work shall be subject to inspection by Permit Sonoma and must conform to all applicable code requirements and approved improvement plans prior to the issuance of a certificate of occupancy.

Based on this uniformly applied regulatory process, the project would not expose people to substantial risk of injury from seismic shaking, and the potential impact is less than significant.

Significance Level

Less than Significant

⁹ California Department of Conservation, "EQ Zapp: California Earthquake Hazards Zone Application", May 15, 2020, <https://maps.conservation.ca.gov/cgs/EQZApp/app/>

iii. Seismic-related ground failure, including liquefaction?

Comment

Strong ground shaking can result in liquefaction, the sudden loss of shear strength in saturated sandy material, resulting in ground failure. Areas of Sonoma County most at risk of liquefaction are along San Pablo Bay and in alluvial valleys. According to the Sonoma County Hazard Mitigation Plan¹⁰, most of project site is located in an area of low susceptibility to liquefaction (Figure 8.1). A geotechnical report prepared by Reese & Associates in January 2019 found that the site can be used for the proposed construction. The report addresses site specific construction recommendations for site preparation and grading, foundation support, slab-on-grade, pavement thickness, geotechnical drainage, and retaining and loading dock wall design; and it will be reviewed by Permit Sonoma engineering staff during the building permit plan check process. As stated above, new structures are subject to engineering standards of the California Building Code, which require that the project meet all standard seismic and soil test/compaction requirements. Therefore, the potential impact from liquefaction would be less than significant.

Significance Level

Less than Significant

iv. Landslides?

Comment

Steep slopes characterize much of Sonoma County, particularly the northern and eastern portion of the County. Where these areas are underlain by weak or unconsolidated earth materials landslides are a hazard. The site is relatively flat, according to the Sonoma County Hazard Mitigation Plan (Figure 8.11), the project site has a Landslide Susceptibility Class of O and is not located in a designated Landslide Hazard Area.¹¹

Significance Level

Less than Significant

b) Result in substantial soil erosion or the loss of topsoil?

Comment

The project involves grading, cuts and fills which require the issuance of a grading permit. Improper grading, both during and post construction, has the potential to increase the volume of runoff from a site which could have adverse downstream flooding and further erosional impacts, and increase soil erosion on and off site which could adversely impact downstream water quality.

Erosion and sediment control provisions of the Drainage and Storm Water Management Ordinance (Chapter 11, Sonoma County Code) and Building Ordinance (Chapter 7, Sonoma County Code) require implementation of flow control best management practices to reduce runoff. Required inspection by Permit Sonoma staff insures that all grading and erosion control measures are constructed according to the approved plans. These ordinance requirements and adopted best management practices are specifically designed to maintain potential water quantity impacts at a less than significant level during and post construction.

Significance Level

Less than Significant

¹⁰ "2016 Sonoma County Operational Area Hazard Mitigation Plan", Sonoma County Permit and Resource Management Department, and Fire and Emergency Services Department, September 2017

¹¹ Ibid

- c) **Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?**

Comment

The project site is subject to seismic shaking and other geologic hazards as described in item 6.a.ii, iii, and iv, above. However, site specific geologic investigation will be conducted through the site development permitting process, which require construction techniques that account for site specific conditions. The project site is not located within a designated Landslide Hazard Area, and is generally flat. Therefore, the potential impact from landslides or liquefaction would be less than significant

Significance Level

Less than Significant

- d) **Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial direct or indirect risks to life or property?**

Comment

Table 18-1-B of the Uniform Building Code is an index of the relative expansive characteristics of soil as determined through laboratory testing. According to the National Resources Conservation Service Soil Survey of Sonoma County¹², soils on the project site consist of Huichica loam, ponded, 0 to 5 percent slopes and Huichica loam, shallow, ponded, 0 to 5 percent slopes. Huichica loam has moderate shrink-swell potential. The Reese & Associates investigation found that the site has moderate to possibly highly expansive clayey soils, which can undergo strength and volume changes with seasonal changes in moisture content¹³. These types of soils must be covered with a moisture confining and protecting blanket of imported soils with low expansion potential. Specific recommendations for site preparation, grading, and compaction were provided in the report, and will be reviewed by Permit Sonoma engineering staff during the building permit plan check process. As stated above, new structures are subject to engineering standards of the California Building Code, including standard seismic and soil test/compaction requirements, therefore the potential building failure impact related to expansive soils would be less than significant.

Significance Level

Less than Significant

- e) **Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?**

Comment

The project will not use a septic system to dispose of wastewater. The project will connect to the Airport/Larkfield/Wikiup Sanitation District (Sonoma Water).

Significance Level

Less than Significant Impact

8. GREENHOUSE GAS EMISSIONS

¹² NRCS Soils Survey of Sonoma County

¹³ Fioni, Dan J., and Jeffrey K. Reese, "Soil Investigation, NeilMed Warehouse, 685 Aviation Boulevard, Santa Rosa, California", Reese & Associates, January 18, 2019

Would the project:

- a) **Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?**

Comment

A Climate Action 2020 Plan was developed by the Sonoma County Regional Climate Plan Authority (RCPA) in 2016 but was unable to be formally adopted due to litigation. The Sonoma County Board of Supervisors adopted a Climate Change Action Resolution on May 8, 2018 which acknowledged the Climate Action 2020 Plan and resolved to "...work towards the RCPA's countywide target to reduce GHG emissions by 40% below 1990 levels by 2030 and 80% below 1990 levels by 2050" as well as adopting twenty goals for reducing GHG emissions including increasing carbon sequestration, increasing renewable energy use, and reducing emissions from the consumption of goods and services. The Bay Area Air Quality Management District (BAAQMD) has published greenhouse gas significance thresholds for use by local governments in the report titled California Environmental Quality Act Air Quality Guidelines May 2017. For projects other than stationary sources, the greenhouse gas significance threshold is 1,100 metric tons per year of CO₂e or 4.6 metric tons of CO₂e per service population (residents and employees) per year. Using the report's screening criteria, the 59,066 square-foot warehouse is below the applicable screening criteria in Table 2-1 of the report for operational criteria air pollutants (864,000 square feet) and operational GHG (64,000 square feet).

For construction activities, the greatest source of greenhouse gas emissions would be diesel emissions from heavy equipment associated with grading and hardscape construction. The BAAQMD does not include a threshold of significance for construction-related GHG emissions. However, the project is below the BAAQMD report's screening criteria for construction related impacts (259,000 square feet). Emissions would be further reduced to a less than significant level with implementation of Mitigation Measure AIR-1, which requires that idling time of diesel-powered construction equipment be limited to five minutes.

Significance Level

Less than Significant Impact

- b) **Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?**

Comment

The County has adopted General Plan Objective OSRC-14.4 which states "Reduce greenhouse gas emissions by 25% below 1990 levels by 2015. In May 2018, the Board of Supervisors adopted a Resolution of Intent to Reduce Greenhouse Gas Emissions that included adoption of the Regional Climate Protection Agency's goal to further reduce greenhouse gas emissions by 40% below 1990 levels by 2030 and by 80% below 1990 levels by 2050. The Resolution of Intent included specific measures that can further reduce greenhouse gas emissions. All new development is required to evaluate all reasonably feasible measures to reduce greenhouse gas emissions and enhance carbon sequestration. The project will not conflict with applicable goals, objectives, plans, policies, or regulations provided mitigation measures specified below are implemented.

Significance Level

Less than Significant with Mitigation Incorporated

Mitigation

Mitigation GHG-1: The applicant shall submit a Greenhouse Gas Reduction Plan for PRMD review and approval that defines measures to reduce greenhouse gas emissions in the design, construction,

and long-term operations of the project. The Greenhouse Gas Reduction Plan shall include all reasonably feasible measures to reduce greenhouse gas emissions to the maximum extent feasible. Measures that must be evaluated include but are not limited to best available conservation technologies for all energy and water uses, installation of renewable energy facilities to meet demand on-site, provisions of electric vehicle charging stations, bicycle facilities including secure bike parking, and lockers and showers for employees, employing best management practices for carbon sequestration, such as no till soils, reduced use of fertilizers, etc.

Monitoring GHG-1: Permit Sonoma staff shall ensure that the methods selected in the Greenhouse Gas Emissions Reduction Plan are listed on all site alteration, grading, building or improvement plans prior to issuance of grading or building permits. Building/grading permits shall not be approved for issuance by Project Review Staff until the Greenhouse Gas Reduction Plan has been approved and incorporated into the design and construction documents for the project.

9. HAZARDS AND HAZARDOUS MATERIALS

Would the project:

- a) **Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?**

Comment

Construction of the project may involve the intermittent transport, storage, use and disposal of potentially hazardous materials, including fuels and lubricants, paints, solvents, and other materials commonly used in construction. During construction activities, any on-site hazardous materials that may be used, stored, or transported would be required to follow standard protocols (as determined by the U.S. EPA, California Department of Health and Safety, and Sonoma County) for maintaining health and safety. Improper transit, storage, or handling of these materials could result in spills. This potential impact would be reduced to a less than significant level with implementation of standard approved construction methods for handling hazardous materials. Therefore, the potential environmental impact associated with the routine transport, use, or disposal of hazardous materials would be less than significant.

Significance Level

Less than Significant

- b) **Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?**

Comment

Potential impacts related to construction and operational use of hazardous materials would be minimized through review and oversight of multiple jurisdictional entities requiring permits for use of hazardous materials, as described in 8.a. above. Therefore, the potential impact from accidents involving the use of hazardous materials would be less than significant.

Significance Level

Less than Significant

- c) **Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?**

Comment

The project does not involve the routine use or transport of hazardous materials and there are no schools within 0.25 miles.

Significance Level

Less than Significant

- d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?**

Comment

There are no known hazardous materials sites within or adjacent to the project limits, based on a review of the following databases on December 15, 2020:

1. The State Water Resources Control Board Geotracker database¹⁴,
2. The Department of Toxic Substances Control EnviroStor database¹⁵ (formerly known as Calsites), and
3. The Calrecycle Solid Waste Information System (SWIS)¹⁶.

The closest hazardous materials sites on record are several LUST (Leaking Underground Storage Tank) cleanup sites no closer than one-quarter mile from the project site. One of the nearest cleanup site cases was closed in 2004. EnviroStor listed a site approximately 0.30 miles northeast of the project ("Ecodyne Pond") that previously manufactured lumber and wood products. Between 1985 and 1998, PAHs, lead, and dioxin were identified in the soils at the site. A Remedial Action Plan (RAP) has since been developed and is in effect, with oversight by the State Department of Toxic Substances Control and North Coast Regional Water Quality Control Board. Due to the distance between this site and the project site, no impacts are expected.

Significance Level

No Impact

- e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?**

Comment

Charles M. Schulz – Sonoma County Airport is roughly 0.50 miles away from the project site. The site is within the Traffic Pattern Zone-B (TPZ-B), as designated by the Sonoma County Comprehensive Airport Land Use Plan, which has a maximum population density of 300 persons per acre. The project would have around 20 employees, and most operations would occur indoors except for the unloading and loading of product at the outdoor dock. Therefore the project would not expose warehouse employees to aircraft related safety hazards or excessive noise.

Significance Level

No Impact

¹⁴ State Water Resources Control Board Geotracker, "Geotracker", State of California, Accessed December 15, 2020, <https://geotracker.waterboards.ca.gov/>

¹⁵ Department of Toxic Substances Control Envirostor, "Envirostor", State of California, Accessed December 15, 2020, <https://www.envirostor.dtsc.ca.gov/public/>

¹⁶ CalRecycle, "Solid Waste Information System", Accessed December 15, 2020, <https://www2.calrecycle.ca.gov/SWFacilities/Directory/>

f) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

Comment

The project would not impair implementation of, or physically interfere with the County's adopted emergency operations plan. There is no separate emergency evacuation plan for the County. The project would not change existing circulation patterns, would not generate substantial new traffic, and therefore, would have no effect on emergency response routes. Refer to section 16 for a discussion of project traffic.

Significance Level

No Impact

g) Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

Comment

According to the Wildland Fire Hazard Area map (Figure PS-1g) in the Sonoma County General Plan, the project site is located in the Local Responsibility Area, over 2 miles from a State Responsibility Area, and is designated as a Moderate Fire Hazard Severity Zone. Moderate Zones are generally located in grasslands and valleys, away from significant forested or chaparral wildland vegetation, as is the case with the project site. Projects located in High and Very High Fire Severity Zones are required by state and county code to have a detailed vegetation management plan developed and reviewed by the Sonoma County Fire Prevention Division before a building permit can be issued. This requirement does not apply to projects located in a Moderate Zone. However, all construction projects must comply with Sonoma County Fire Safety Ordinance (Chapter 13), with requirements such as installing fire sprinklers in buildings, providing emergency vehicle access, and maintaining a dedicated fire-fighting water supply on-site. Application of County standards reduces the project's potential to expose people or structures to a significant risk of loss, injury or death involving wildland fires to a less than significant level.

Significance Level

Less than Significant

10. HYDROLOGY AND WATER QUALITY

Would the project:

a) Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or groundwater quality?

Comment

Construction of the warehouse building and associated hardscape would result in grading and ground disturbance of 1.2 acres. Construction activities, completed improvements, and project operations could all affect the quantity and/or quality of stormwater runoff.

Site drainage occurs by sheet flow to the northwestern portion of the site. the south. Besides a depression in the northern portion of the sight, the site is generally flat. There are no immediate stream channels adjacent to the property; the closest drainage is 300 feet north of the project boundaries. However, a number of regulations could apply to the project, summarized below.

A construction project disturbing one or more acres of soil is required to obtain coverage under the State Water Resources Control Board (SWRCB) Construction General Permit Order 2009-0009-DWQ for Discharges of Storm Water Runoff Associated with Construction Activity¹⁷. Construction activities subject to this permit include clearing, grading, stockpiling, excavation, and reconstruction of existing facilities involving removal and replacement. The General Permit requires submittal of a Notice of Intent (NOI) package, and development and implementation of a Storm Water Pollution Prevention Plan (SWPPP) which, in addition to other requirements, must include Best Management Practices (BMPs) to protect the quality of stormwater runoff.

Sonoma County also requires project applicants to prepare a grading and drainage plan (Erosion Prevention and Sediment Control Plan) in conformance with Chapter 11 (Construction Grading and Drainage Ordinance) and Chapter 11A (Storm Water Quality Ordinance) of the Sonoma County Code. The project would involve placement of more than 10,000 square feet of impervious surface area located in an area subject to the North Coast RWQCB Municipal Separate Storm Sewer Systems (MS4) Permit. Therefore, it must both meet the requirements of the Sonoma County Storm Water Quality Ordinance and incorporate Low Impact Development (LID) Best Management Practices (BMPs) contained in the City of Santa Rosa and County of Sonoma Storm Water Low Impact Development Technical Design Manual to address construction and post construction water quality requirements. Required inspections by Permit Sonoma staff ensure that all grading and erosion control measures are constructed according to the approved plans.

A Preliminary Stormwater Low Impact Development (SWLID) report¹⁸ was submitted with the project, then reviewed and approved by the Grading and Stormwater Division of Permit Sonoma. A final SWLID submittal will be required prior to issuance of grading or building permits, which will ensure the project will incorporate all LID BMPs.

All of the above requirements and adopted best management practices are specifically designed to maintain potential water quality impacts at a less than significant level during and post construction.

Significance Level

Less than Significant

b) Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?

Comment

The site is located in a Groundwater Availability Class 1 – Major Groundwater Basin, and is within the Santa Rosa Valley Priority basin defined under the Sustainable Groundwater Management Act (SGMA). However, the project does not rely on groundwater; municipal water service is provided by the Town of Windsor. The project will not capture precipitation or impede overland stormwater flows in a way that might interfere with groundwater recharge.

Significance Level

Less than Significant

¹⁷ State Water Resources Control Board, “2009-0009-DWQ CONSTRUCTION GENERAL PERMIT”, California Environmental Protection Agency, September 26, 2018, https://www.waterboards.ca.gov/water_issues/programs/stormwater/constpermits.shtml

¹⁸ Bartholomew, Paul V., “SWLID Report, Neilmed Warehouse, 685 Aviation Boulevard, Santa Rosa, CA”, Brelje & Race, December 20, 2019

c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which:

i. would result in substantial erosion or siltation on- or off-site?

Comment

There are no streams, drainage channels, or wetland features on the project site. Site drainage occurs by sheet flow to the northwest. Construction of the proposed project would involve cuts, fills, and other grading. Unregulated grading during construction has the potential to increase soil erosion from a site. Construction grading activities would be subject to a grading permit, which requires installation of adequate stormwater treatment measures to prevent soil erosion during construction, such as silt fencing, straw wattles, and soils discharge controls at construction site entrance(s). Compliance with the County grading regulations is aimed at capturing and treating all project runoff onsite, thereby reducing the potential for soil erosion and sediment delivery from the site. The ordinance requirements and BMPs are specifically designed to maintain water quantity and ensure erosion and siltation impacts are less than significant during and post construction.

Significance Level

Less than Significant

ii. substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite;

Comment

The project will increase the amount of impervious surface area on the site. Prior to grading or building permit issuance, construction details for all post-construction storm water Best Management Practices (BMPs) shall be submitted for review and approval by the Grading & Storm Water Section of Permit Sonoma. Post-construction storm water BMPs must be installed per approved plans and specifications, and working properly prior to finalizing the grading or building permits. They shall be designed and installed pursuant to the adopted Sonoma County Best Management Practice Guide. BMPs would prevent the alteration of site drainage, or increase in surface runoff and avoid flooding. Project Low Impact Development techniques would include limiting impervious surfaces, dispersing development over larger areas, and creation of storm water detainment areas. Post construction storm water BMPs include filtering, settling, or removing pollutants. Through standard permitting requirements, potential flooding impacts are reduced to a less than significant level.

Significance Level

Less than Significant

iii. create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or

Comment

On-site construction would result in new impervious surface and generation of stormwater. Bioretention swales are proposed to manage stormwater drainage and retain all, if not most, stormwater on the site. The project would require a grading permit, which would not be issued until all required stormwater treatment options have been incorporated in compliance with all applicable standards of the County Code.

Significance Level

Less than Significant

iv. impede or redirect flood flows?

Comment

The site is not located in a 100-year flood plain where construction of new structures could impede or redirect flood flows.

Significance Level

No Impact

d) In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?

Comment

The project site is not located in a 100-year flood zone or Special Flood Hazard Area, as determined by the Federal Emergency Management Agency (FEMA). Nor is the site in an area subject to seiche or tsunami. A seiche is a wave in a large enclosed or partly enclosed body of water triggered by an earthquake. The project site is not located near enough to a large body of water or the coastline to be subject to earthquake-triggered waves.

Significance Level

No Impact

e) Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

Comment

The project is subject to Chapter 11 (Construction Grading and Drainage Ordinance) and Chapter 11A (Storm Water Quality Ordinance) of the Sonoma County Code and the Sonoma County Storm Water Low Impact Development Guide, all of which include performance standards and Best Management Practices for pre-construction, construction, and post-construction to prevent and/or minimize the discharge of pollutants, including sediment, from the project site. The site is located within the Santa Rosa Valley Priority SGMA basin, an area that requires monitoring and reporting of groundwater use. However, the project does not rely on groundwater; municipal water service is provided by the Town of Windsor. The project will not impede or conflict with implementation of the Sonoma County Storm Water Low Impact Development Guidelines or the goals of the Sustainable Groundwater Management Act.

Significance Level

No Significant

11. LAND USE AND PLANNING

Would the project:

a) Physically divide an established community?

Comment

The project would not physically divide a community. The project would not involve construction of a physical structure (such as a major transportation facility) or removal of a primary access route (such as a road or bridge) that would impair mobility within an established community or between a community and outlying areas. No impact would occur.

Significance Level

No Impact

b) Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

Comment

The General Plan Land Use designation is Limited Industrial and the Zoning District is MP (Industrial Park). The proposed warehouse use is compatible with General Plan policies and Zoning Code requirements for industrial areas. The project site is located within the boundaries of the Airport Industrial Area Specific Plan. It is not within a designated open space or scenic resource area, therefore, no such policies or regulations apply to the project. The site is located within a designated Valley Oak Habitat protection area. However, no valley oak habitat occurs on the parcel. By implementing the mitigation measures identified in this document, the project would not conflict with any applicable land use plan adopted for the purpose of avoiding or mitigating an environmental effect.

Significance Level

No Impact

12. MINERAL RESOURCES

Would the project:

a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

Comment

Sonoma County has adopted the Aggregate Resources Management Plan that identifies aggregate resources of statewide or regional significance (areas classified as MRZ-2 by the State Geologist). The project site is not located within a known mineral resource deposit area, according to the Sonoma County Aggregate Resources Management Plan, as amended in 2010.

Significance Level

No Impact

b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

Comment

The project site is not located within an area of locally-important mineral resource recovery site and the site is not zoned MR (Mineral Resources). No locally-important mineral resources are known to occur at the site.

Significance Level

No Impact

13. NOISE

Would the project:

- a) **Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?**

Comment

County noise standards for non-transportation operational noise are provided in Table NE-2 of the General Plan (Table 3 below). These thresholds may be adjusted based on site-specific conditions, such as a very high or very low ambient noise level, specific types of noise (e.g., dog barking, simple tone noises), or short-term noise sources permitted to occur no more than six days per year (e.g., concerts, special events).

Table 2. Maximum Allowable Exterior Noise Exposures for Non-transportation Noise Sources

Hourly Noise Metric ¹ , dBA	Daytime (7 a.m. to 10 p.m.)	Nighttime (10 p.m. to 7 a.m.)
L50 (30 minutes in any hour)	50	45
L25 (15 minutes in any hour)	55	50
L08 (4 minutes 48 seconds in any hour)	60	55
L02 (72 seconds in any hour)	65	60
¹ The sound level exceeded n% of the time in any hour. For example, the L50 is the value exceeded 50% of the time or 30 minutes in any hour; this is the median noise level.		

Sensitive receptors include hospitals, schools, convalescent facilities, and residential areas. The site is in the middle of the Airport Business Park, with no nearby residential areas, schools, or hospitals. No outdoor activities are allowed with this project with the exception of loading/unloading supplies and materials at the outdoor loading dock, which may involve the use of forklifts with backup alarms. Therefore, during project operations there would be temporary periods of relatively short duration where vehicle generated noise occurs when items are delivered to or removed from the warehouse. Hours of operation for the warehouses are 5:00am to 12:00 am. However, County standard project conditions of approval identify specific General Plan land use standards and require they be met, which would reduce these impacts to less than significant levels. No significant operational noise impacts are anticipated.

Neither the General Plan, nor the County Code establishes any noise thresholds or standards for temporary construction activities. Short-term construction activities would increase ambient noise levels at the project site and vicinity, but would subside once construction of the proposed project is completed. Mitigation Measure NOISE-1 would reduce the temporary construction noise impact to a less than significant level.

Significance Level

Less than Significant with Mitigation Incorporated

Mitigation

Mitigation Measure NOISE-1: All plans and specifications or construction plans shall include the following notes:

- a) All internal combustion engines used during construction of this project will be operated with mufflers that meet the requirements of the State Resources Code, and, where applicable, the Vehicle Code. Equipment shall be properly maintained and turned off when not in use.
- b) Except for actions taken to prevent an emergency, or to deal with an existing emergency, all construction activities shall be restricted to the hours of 7:00 a.m. and 7:00 p.m. (use this if no nearby receptors, or 5:00 pm if nearby receptors) on weekdays and 9:00 a.m. and 7:00 p.m. (same note as above) on weekends and holidays. If work outside the times specified above becomes necessary, the applicant shall notify the Permit Sonoma Project Review Division as soon as practical.
- c) There will be no start up of machines nor equipment prior to 7:00 a.m, Monday through Friday or 9:00 am on weekends and holidays; no delivery of materials or equipment prior to 7:00 a.m nor past 7:00 p.m, (same note as above) Monday through Friday or prior to 9:00 a.m. nor past 7:00 p.m. on weekends and holidays and no servicing of equipment past 7:00 p.m., Monday through Friday, or weekends and holidays. A sign(s) shall be posted on the site regarding the allowable hours of construction, and including the developer- and contractors mobile phone number for public contact 24 hours a day or during the hours outside of the restricted hours.
- d) Pile driving activities shall be limited to 7:30 a.m. to 7:00 p.m. weekdays only (same note as above).
- e) Construction maintenance, storage and staging areas for construction equipment shall avoid proximity to residential areas to the maximum extent practicable. Stationary construction equipment, such as compressors, mixers, etc., shall be placed away from residential areas and/or provided with acoustical shielding. Quiet construction equipment shall be used when possible.
- f) The developer shall designate a Project Manager with authority to implement the mitigation prior to issuance of a building/grading permit. The Project Managers 24-hour mobile phone number shall be conspicuously posted at the construction site. The Project Manager shall determine the cause of noise complaints (e.g. starting too early, faulty muffler, etc.) and shall take prompt action to correct the problem.

Monitoring

Monitoring NOISE-1: Permit Sonoma Project Review Division staff shall ensure that the measures are listed on all site alteration, grading, building or improvement plans, prior to issuance of grading or building permits. Permit Sonoma staff shall inspect the site prior to construction to assure that the signs are in place and the applicable phone numbers are correct. Any noise complaints will be investigated by Permit Sonoma staff. If violations are found, Permit Sonoma shall seek voluntary compliance from the permit holder, or may require a noise consultant to evaluate the problem and recommend corrective actions, and thereafter may initiate an enforcement action and/or revocation or modification proceedings, as appropriate.

b) Generation of excessive groundborne vibration or groundborne noise levels?

Comment

The project would include construction activities that may generate minor ground borne vibration and noise from conventional construction equipment, but no intensive vibratory noise would occur, such as pile-driving. All construction noise would be short-term, temporary, and limited to daytime hours. There are no other activities or uses associated with the project that would expose persons to or generate excessive ground borne vibration or ground borne noise levels.

Significance Level

Less than Significant

c) For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?

Comment

The Charles M. Schluz-Sonoma County Airport is roughly a half-mile west of the site. The Sonoma County Comprehensive Airport Land Use Plan (CALUP) identifies warehousing as an acceptable or compatible land use near airport operations. The warehouse would have around 20 employees, and outdoor activities would be limited to loading/unloading of supplies.

Significance Level

Less than Significant Impact

14. POPULATION AND HOUSING

Would the project:

a) Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?

Comment

The proposed project does not propose new housing, nor would it generate significant new demand for housing in the area (20 employees are anticipated). This small increase in employment opportunities is not anticipated to result in an indirect increase in population as it is anticipated that employees would be existing residents of the Bay Area. No new major infrastructure is proposed. Therefore, the project would not induce substantial population growth.

Significance Level

Less than Significant

b) Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?

Comment

No housing would be displaced by the project and no replacement housing would be required.

Significance Level

No Impact

15. PUBLIC SERVICES

Would the project:

- a) **Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:**

i. Fire protection?

Comment

The proposed project is within the service area of the Sonoma County Fire Protection District. The Sonoma County Protection District would continue to serve this area, and existing fire protection facilities are anticipated to be adequate. Sonoma County Code requires that all new development meet Fire Safe Standards (Chapter 13). The County Fire Marshal reviewed the project referral and provided conditions of approval to comply with Fire Safe Standards, including fire truck access improvements and fire protection methods such as sprinklers in buildings, alarm systems, extinguishers, vegetation management, hazardous materials management and management of flammable or combustible liquids and gases. None of the conditions required construction of new or expanded fire protection facilities. Therefore, the project would not necessitate or facilitate construction of new fire protection facilities in order to maintain acceptable service ratios or response times.

Significance Level

Less than Significant

ii. Police?

Comment

The Sonoma County Sheriff would continue to serve this area; existing sheriff protection facilities are anticipated to be adequate. The proposed project does not propose new housing. The estimated 20 new job opportunities would not be anticipated to result in a substantial number of new residents moving to the area and requiring police protection. Therefore, the project would not necessitate or facilitate construction of new police protection facilities resulting in environmental impacts in order to maintain acceptable service ratios or response times.

Significance Level

Less than Significant

iii. Schools?

Comment

Development fees to offset potential impacts to public services, including school fees, are required by Sonoma County Code and state law for new subdivisions and residential developments. The project does not involve residential development, and the estimated 20 new job opportunities would not be anticipated to result in a substantial number of new residents moving to the area and requiring additional school facilities. Therefore, the project would not necessitate or facilitate construction of new schools resulting in environmental impacts in order to maintain acceptable service ratios.

Significance Level

Less than Significant

iv. Parks?

Comment

Development fees to offset potential impacts to public services, including parks fees, are required by Sonoma County Code and state law for new subdivisions and residential developments. The project does not involve residential development, and the estimated 20 new job opportunities would not be anticipated to result in a substantial number of new residents moving to the area and requiring additional park facilities. Therefore, the project would not necessitate or facilitate construction of new parks resulting in environmental impacts in order to maintain acceptable service ratios.

Significance Level

Less than Significant

v. Other public facilities?

Comment

The project will connect to the Airport/Larkfield/Wikiup Sanitation District (Sonoma Water) for sewer service and the Town of Windsor for water. An estimated 20 employees for a dry goods warehouse is not anticipated to result in an increase in water demand or wastewater processing capacity requiring construction of new facilities. No other public facilities are anticipated to be required as a result of the project.

Significance Level

Less than Significant

16. RECREATION

Would the project:

- a) **Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?**

Comment

The proposed project does not propose new housing, nor would it generate significant new demand for housing in the area (up to 20 new employees are anticipated). Therefore, the project would not cause or accelerate substantial physical deterioration of parks or recreational facilities, and would have no impact on the use of existing neighborhood and regional parks or other recreational facilities.

Significance Level

Less than Significant

- b) **Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?**

Comment

The project does not involve the construction or expansion of recreational facilities.

Significance Level

No Impact

17. TRANSPORTATION

Would the project:

- a) **Conflict with a program, plan, ordinance or policy addressing the circulation system, including transit, roadways, bicycle and pedestrian facilities?**

Comment

The warehouse project is anticipated to require 20 employees. As proposed, it would house products that would be shipped from 551 Aviation Boulevard (under the same ownership), allowing local products to be stored nearby rather than trucked out of the area for storage. The project is therefore not expected to add truck traffic to the area because trips leaving the project warehouse would replace trips currently moving the products to storage facilities out of the area. However, for analysis purposes all trips were treated as new trips. A focused traffic study, dated December 28, 2020¹⁹ and prepared by W-Trans, found that the project is anticipated to generate 103 new vehicle trips per day, including 10 a.m. peak hour and 11 p.m. peak hour trips.

The Sonoma County General Plan includes objectives for maintaining an acceptable Level of Service (LOS) for the countywide roadway system. General Plan Objectives CT-4.1 and CT-4.2 define an acceptable Level of Service as LOS C or better for roadway segments, and LOS D or better at roadway intersections. The Traffic Impact Study for the Hyatt Place Sonoma Wine Country project²⁰ at 3750 North Laughlin Road, just south of Airport Boulevard, found that the five intersections on Airport Boulevard from US 101 North to North Laughlin Road-Skylane Boulevard are identified as operating at LOS C or better under existing condition. Under 2040 volumes, all the intersections are projected to operate at LOS C or better except for Airport Boulevard/N. Laughlin Road-Skylane Boulevard, which would operate at LOS F. However, this intersection is included on the County's list of locations that are planned to be signalized, and with a signal operation would improve to LOS C. Because LOS C operation is reasonably expected under Future volumes and was documented for existing conditions, the project will not conflict with the County's LOS objectives.

The nearest transit stop, located at Aviation Boulevard/Skylane Boulevard, is served by Sonoma County Transit Route 62, which runs to Windsor Depot to the north and Santa Rosa Transit Mall to the south. The buses for this route are currently operating on their weekday schedule with service from approximately 7:30 a.m. to 7:45 p.m. with headways of one to two hours. The transit stop is within one-quarter of a mile walking distance, which is considered a "walkable" distance. Existing transit facilities are adequate to accommodate the project.

Bicycle lanes do not currently exist on Aviation Boulevard or Skylane Boulevard. However, the 2010 Sonoma County Bicycle and Pedestrian Plan proposes a Class II bike lane along the Skylane Boulevard. This project would not interfere or conflict with the proposed bike lane. The project proposes 7 bicycle parking spaces to encourage bicycle transportation in the future.

Sidewalks do not currently exist along the frontage Aviation Boulevard, off of where the project site is located. Given the lack of sidewalk on Aviation Boulevard and on other frontages abutting the private driveway leading to the project site, coupled with the lack of pedestrian destinations, the installation of sidewalks is not warranted.

¹⁹ Kim, Jade and Dalene J. Whitlock, "Focused Traffic Study for the 685 Aviation Boulevard Project", W-Trans, December 28, 2020

²⁰ Whitlock, J., Dalene, "Traffic Impact Study for the Hyatt Place Sonoma Wine Country", W-Trans, April 29, 2020

Significance Level

Less than Significant

b) Conflict or be inconsistent with CEQA Guidelines Section 15064.3, subdivision (b)?

Comment

Pursuant to CEQA Guidelines Section 15064.3, subdivision (b), and applicable starting July 1, 2020, Vehicle Miles Traveled (VMT) is now the appropriate metric to evaluate transportation impacts of land use projects, superseding use of the measure of traffic congestion (i.e. Level of Service). To assist with implementation of the new CEQA practice, the Sonoma County Transportation Agency (SCTA) is in the process of developing screening and modeling tools for local jurisdictions. In the interim, the Technical Advisory provided by the Governor's Office of Planning and Research offers a threshold to screen out smaller projects from further analysis. Absent substantial evidence otherwise or inconsistency with a general plan, 110 daily vehicle trips may be assumed to have a less than significant transportation impact²¹.

A focused traffic study, dated December 28, 2020 and prepared by W-Trans, found that the project is anticipated to generate 103 new vehicle trips per day, including 10 a.m. peak hour and 11 p.m. peak hour trips²². Additionally, many of these trips are expected to have a very short length associated with trucks carrying finished goods between the production and storage facilities. Because 103 trips is below the screening threshold, the project is consistent with the General Plan, including Level of Service objectives, and there is no other evidence suggesting otherwise, the project is anticipated to have a less than significant impact on vehicle miles traveled.

Significance Level

Less than Significant

c) Substantially increase hazards due to geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

Comment

The project would not increase hazards because it does not modify alignment of existing public roadways. Brelje & Race prepared an exhibit²³ demonstrating the possible turning movements and circulation of trucks visiting the project site. Trucks will access the site from the private driveway off of Aviation Boulevard, turn to the east and then back into the loading dock. On departure, they can pull ahead and continue along the private driveway east to Brickway Boulevard. All warehouse activities would take place inside the parcel interior. No equipment would impact public roads, except for temporary construction-related vehicle impacts, which would cease upon completion of project construction and would be considered a less than significant impact.

Significance Level

Less than Significant Impact

d) Result in inadequate emergency access?

²¹ Governor's Office of Planning and Research, "Technical Advisory on Evaluating Transportation Impacts in CEQA", State of California, December 2018

²² Kim, Jade and Dalene J. Whitlock, "Focused Traffic Study for the 685 Aviation Boulevard Project", W-Trans, December 28, 2020

²³ "Windsor Fire Truck Turning Template", Brelje & Race, August 9, 2018

Comment

Emergency vehicles can access the project site through the driveway coming from Aviation Boulevard. Brelje & Race prepared an emergency vehicle turnaround exhibit²⁴ demonstrating that onsite circulation can accommodate the turning movements of a standard sized emergency vehicle.

Significance Level

Less than Significant

f) Result in inadequate parking capacity?

Comment

The Sonoma County Zoning Code requires one parking space per 2000 square feet of warehouse space, and one space per 250 square feet. The project is a 59,066 square-foot warehouse with 500 square feet of the total reserved as office space, therefore 32 spaces are required.

The development would have 43 spaces onsite. Fourteen of those spaces are shared with the property to the south, therefore 29 would specifically be designated for onsite uses. The Sonoma County Design Review Committee has provided preliminary approval for a minor reduction in required parking of up to 20 percent due to the project's provision of two electric vehicle charging stations, and three spaces reserved for clean air or vanpool vehicles. A minimum of 26 spaces would be required after the reduction, which the project meets. Further, it is assumed that some of the 14 shared spaces will be available for onsite needs.

Significance Level

No Impact

18. TRIBAL CULTURAL RESOURCES

a) Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California native American tribe, and that is:

i) Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5030.1(k), or

Comment

As discussed in section 5(a), Tom Origer & Associates conducted a cultural resources evaluation of the project site²⁵. There are no known resources on site, but construction related to the project could uncover such materials. Mitigation Measure CUL-1 will reduce potential impacts to less than significant.

Significance Level

Less than Significant with Mitigation Incorporated

²⁴ "WB-65 Truck Turning Template", Brelje & Race, March 21, 2019

²⁵ Marella, Elena, B.A. and Thomas M. Origer, M.A., "Cultural Resources Study of the Property at 685 Aviation Boulevard, Santa Rosa, Sonoma County, California", Tom Origer and Associates, December 20, 2019

Mitigation

See Mitigation Measure and Monitoring CUL-1

ii) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code § 5024.1. In applying the criteria set forth in subdivision (c) of Public Resource Code § 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

Comment

See section 5 and section 18(a)(i). Mitigation Measure CUL-1 will reduce potential impacts to less than significant.

Significance Level

Less than Significant with Mitigation Incorporated

Mitigation

See Mitigation Measure and Monitoring CUL-1

19. UTILITIES AND SERVICE SYSTEMS

Would the project:

- a) **Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?**

Comment

All services are already present at the project site. Existing buildings in the business park are already served by Town of Windsor water and the Airport/Larkfield/Wikiup Sanitation District, electrical power through various providers, and telecommunications. No expansion of any public services would be required.

Significance Level

Less than Significant

- b) **Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?**

Comment

Sufficient water would be provided by the Town of Windsor. The Town of Windsor has issued conditions of approval requiring measures for water efficiency. No water supply issues are anticipated.

Significance Level

Less than Significant

- c) **Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?**

Comment

The project would be served by the Airport/Larkfield/Wikiup Sanitation District (Sonoma Water). The project was referred to Sonoma Water, which provided conditions of approval for the project's sewer connection. One of the conditions requires a "Sewer Capacity Study" prior to building permit issuance if determined necessary by Sonoma Water. For a dry goods warehouse with 20 employees, sewer capacity issues are not anticipated.

Significance Level

Less than Significant

- d) Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?**

Comment

Sonoma County has a solid waste management program in place that provides solid waste collection and disposal services for the entire County. The program can accommodate the permitted collection and disposal of the solid waste that would result from the proposed project.

Significance Level

Less than Significant

- e) Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?**

Comment

No applicable federal solid waste regulations would apply to the project. At the State level, the Integrated Waste Management Act mandates a reduction of waste being disposed and establishes an integrated framework for program implementation, solid waste planning, and solid waste facility and landfill compliance. Sonoma County has access to adequate permitted landfill capacity and reduction, reuse, and recycling programs to serve the proposed project. Construction and operational waste generated as a result of the project would require management and disposal in compliance with local and state regulations. The project would not conflict with implementation of such programs.

Significance Level

Less than Significant

20. WILDFIRE

If located in or near state responsibility areas or lands classified as very high fire severity zones, would the project:

- a) Substantially impair an adopted emergency response plan or emergency evacuation plan?**

Comment

The project site is not located in or within 2 miles of the State Responsibility Area or a High Fire Hazard Severity Zone. The project is located in a Moderate Fire Hazard Severity Zone within a developed industrial area near the city limits of Windsor. No significant impacts related to wildfire are anticipated.

Significance Level

No Impact

- b) Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?**

Comment

The project site is not located in or within 2 miles of the State Responsibility Area or a High Fire Hazard Severity Zone. The project is located in a Moderate Fire Hazard Severity Zone within a developed industrial area near the city limits of Windsor. No significant impacts related to wildfire are anticipated.

Significance Level

No Impact

- c) Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk of that may result in temporary or ongoing impacts to the environment?**

Comment

The project site is not located in or within 2 miles of the State Responsibility Area or a High Fire Hazard Severity Zone. The project is located in a Moderate Fire Hazard Severity Zone within a developed industrial area near the city limits of Windsor. No significant impacts related to wildfire are anticipated.

Significance Level

No Impact

- d) Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?**

Comment

The project site is not located in or within 2 miles of the State Responsibility Area or a High Fire Hazard Severity Zone. The project is located in a Moderate Fire Hazard Severity Zone within a developed industrial area near the city limits of Windsor. No significant impacts related to wildfire are anticipated.

Significance Level

No Impact

21. MANDATORY FINDINGS OF SIGNIFICANCE

- a) Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?**

Comment

Potential project impacts to special status plant and wildlife species and habitat are addressed in Section 4 with Mitigation Measures BIO-1 through 3. Potential adverse project impacts to cultural and tribal resources are addressed in section 5 and 18. Implementation of the required mitigation measures will reduce potential impacts to a less than significant level. Potentially significant impacts to air quality, greenhouse gas emissions, and noise are reduced to a less than significant level

through implementation of Mitigation Measures AIR-1, GHG-1, and NOISE-1.

Significance Level

Less than Significant with Mitigation Incorporated

Mitigation

See Mitigation Measures and Monitoring AIR-1, BIO-1 through 3, CUL-1, GHG-1, and NOISE-1.

- b) Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?**

Comment

The project would contribute to potential cumulative impacts related to air quality, greenhouse gases, biological resources, cultural resources, noise, and tribal resources. However, no individual project impacts have been identified in this Initial Study that are cumulatively considerable because the impacts of the project and project specific mitigation measures, when considered with past, other current, and probable future projects, would reduce cumulative impacts to less than significant in accordance with the General Plan.

Significance Level

Less than Significant with Mitigation Incorporated

- c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?**

Comment

The proposed project has some potential to cause adverse impacts on human beings, both directly and indirectly. However, all potential impact and adverse effects on humans were analyzed, and would be less than significant with the mitigation measures identified in the Initial Study incorporated into the project.

Significance Level

Less than Significant with Mitigation Incorporated

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SONOMA COUNTY DESIGN REVIEW COMMITTEE
DRAFT Conditions of Approval

Staff:	Katrina Braehmer	Date:	April 7, 2021
Applicant:	Ken DiLillo	File No:	PLP19-0045 (UPE19-0081 and DRH19-0007)
Owner:	Alisha and Natasha Properties, LLC	APN:	059-340-056
Address:	685 Aviation Blvd., Santa Rosa		

Project Description: Request for Design Review and a Use Permit for a new 59,066 square foot, 50-foot tall, two-story warehouse for dry-goods, with 500+/- square feet to be used as office space, on a 1.53-acre parcel located in the Airport Business Center. The warehouse is a permitted use, and a Use Permit is required for additional building height above the 28-foot maximum within the building setback line.

Prior to commencing the use, evidence must be submitted to the file that all of the following non-operational conditions have been met.

GENERAL:

1. Within five working days after project approval, the applicant shall pay a mandatory Notice of Determination filing fee of \$50.00 (or latest fee in effect at time of payment) for County Clerk processing, and \$2,480.25 (or latest fee in effect at time of payment) because a Mitigated Negative Declaration was prepared, for a **total of \$2,530.25 made payable to Sonoma County Clerk** and submitted to PRMD. If the required filing fee is not paid for a project, the project will not be operative, vested, or final and any local permits issued for the project will be invalid (Section 711.4(c)(3) of the Fish and Game Code.) NOTE: If the fee is not paid within five days after approval of the project, it will extend time frames for CEQA legal challenges.
2. Prior to submitting a building permit application, the applicant shall submit to Permit Sonoma a Condition Compliance Review fee deposit (amount to be determined consistent with the ordinance in effect at the time). In addition, the applicant shall be responsible for payment of any additional compliance review fees that exceed the initial deposit (based upon hours of staff time worked) prior to final inspection being granted.
3. This "At Cost" entitlement is not vested until all permit processing costs and development fees are paid in full. Additionally, no grading or building permits shall be issued until all permit processing costs and development fees are paid in full.
4. The applicant shall include these Conditions of Approval on separate sheets of plan sets to be submitted for building and grading permit applications.
5. This use shall be constructed, maintained, and operated in conformance with all applicable county, state, and federal statutes, ordinances, rules, and regulations. A violation of any applicable statute, ordinance, rule or regulation shall be a violation of the Permit, subject to revocation or modification.
6. Construction shall conform to the plans approved by the Sonoma County Design Review Committee. Any proposed modification, alteration, and/or expansion of the use authorized by this Design Review Permit shall require the prior review and approval of Permit Sonoma or the Design Review Committee, as appropriate. Such changes may require a new or modified approval and additional environmental review.
7. The Director of Permit Sonoma is hereby authorized to modify these conditions for minor adjustments to respond to unforeseen field constraints provided that the goals of these conditions can be safely achieved in some other manner. The applicant must submit a written request to

Permit Sonoma demonstrating that the conditions is infeasible due to specific constraints (e.g. lack of property rights) and shall include a proposed alternative measure or option to meet the goal or purpose of the condition. Permit Sonoma shall consult with affected departments and agencies and may require an application for modification of the approved permit. Changes to conditions that may be authorized by Permit Sonoma are limited to those items that are not adopted standards or were not adopted as mitigation measures or that were not at issue during the public hearing process. Any modification of the permit conditions shall be documented with an approval letter from Permit Sonoma, and shall not affect the original permit approval date or the term for expiration of the permit.

The owner/operator and all successors in interest, shall comply with all applicable provisions of the Sonoma County Code and all other applicable local, state and federal regulations.

- 8. This permit shall be subject to revocation or modification by the Board of Zoning Adjustments if: (a) the Board finds that there has been noncompliance with any of the conditions or (b) the Board finds that the use for which this permit is hereby granted constitutes a nuisance. Any such revocation shall be preceded by a public hearing noticed and heard pursuant to Section 26-92-120 and 26-92-140 of the Sonoma County Code.

In any case where this Permit has not been used within two (2) years after the date of the granting thereof, or for such additional period as may be specified in the permit, such permit shall become automatically void and of no further effect, provided however, that upon written request by the applicant prior to the expiration of the two year period the permit approval may be extended for not more than one (1) year by the authority which granted the original permit pursuant to Section 26-92-130 of the Sonoma County Code.

BUILDING:

“The conditions below have been satisfied BY _____ DATE _____

- 9. The applicant shall apply for and obtain building related permits from the Permit and Resource Management Department (PRMD). The necessary applications appear to be, but may not be limited to, site review, building permit, and grading permit.
- 10. Prior to initiation of the approved use, the project shall comply with the accessibility requirements set forth in the most recent California Building Code (CBC), as determined by the PRMD Building Division. Such accessibility requirements shall apply to all new construction and remodeling and, where required by the CBC, to retrofitting of the existing structure.
- 11. The business operator shall post a sign that includes the phone number for a current job manager for the benefit of neighbors. The job manager can be contacted if there are any problems associated with the construction process site such as dust, storm water runoff, hours of operation, equipment noise, traffic issues or lack of compliance with any project conditions of approval.

HEALTH:

“The conditions below have been satisfied BY _____ DATE _____

PRIOR TO PERMIT ISSUANCE

- 12. Connection shall be made to public sewer and water. Prior to building permit issuance and vesting the Use Permit the applicant shall submit a “Will Serve Letter” for water and sewer to the Project Review Health Specialist to verify compliance, except for a connection to a County operated sewer system where clearance for the sewer will come from the PRMD Sanitation Section. Note that Will Serve Letters in contradiction of a moratorium by the appropriate

regulating agency are not acceptable.

13. Prior to building permit issuance, areas within refuse enclosures for commercial buildings with fifteen thousand (15,000) square feet of floor space or greater, food facilities, food retailers, inns and hotels, and multiple housing units with seven (7) or more bedrooms, shall drain to the sanitary sewer system or other appropriately permitted disposal facility, and comply with the following criteria:
 - a. Container areas shall not be less than ten feet (10') wide, seven and one-half feet deep and six feet (6') high.
 - b. Gates, if installed on the container area, shall be double doors, opening at the center and level with the access road.
 - c. The outside perimeter of the trash enclosure shall be graded to prevent storm water from draining into the sanitary sewer system.
 - d. The trash enclosure shall be covered with a roof or awning. Overhangs, wiring or other obstructions on the approach to the container area must be at least thirteen feet six inches (13'6") high and at least sixteen feet (16') high directly over the container area.
 - e. The containers for refuse and recyclables shall be of sufficient size and adequate number to contain without overflowing all of the refuse and recyclables that are generated on the premises during the designated removal period per the Solid Waste Management Plan.

OPERATIONAL

14. A safe, potable water supply shall be provided and maintained.
15. Noise shall be controlled in accordance with Table NE-2 (or an adjusted Table NE-2 with respect to ambient noise as described in General Plan 2020, Policy NE-1c,) as measured at the exterior property line of any affected residential or sensitive land use:

TABLE NE-2: Maximum Allowable Exterior Noise Exposures

Hourly Noise Metric ¹ , dBA	Daytime (7 a.m. to 10 p.m.)	Nighttime (10 p.m. to 7 a.m.)
L50 (30 minutes in any hour)	50	45
L25 (15 minutes in any hour)	55	50
L08 (4 minutes 48 seconds in any hour)	60	55
L02 (72 seconds in any hour)	65	60
¹ The sound level exceeded n% of the time in any hour. For example, the L50 is the value exceeded 50% of the time or 30 minutes in any hour; this is the median noise level. The L02 is the sound level exceeded 1 minute in any hour.		

16. All garbage and refuse on this site shall accumulate or be stored in non-absorbent, water-tight, vector resistant, durable, easily cleanable, galvanized metal or heavy plastic containers with tight fitting lids. No refuse container shall be filled beyond the capacity to completely close the lid. Garbage and refuse on this site shall accumulate or be stored for no more than seven calendar days, and shall be properly disposed of at a County Transfer Station or County Landfill before the end of the seventh day.
17. Smoking is prohibited in any public area, in any dining area, service area (including entry lines or ticket purchase lines) and in any enclosed area that is a place of employment (Sonoma County Code 32-6). "No Smoking" signs shall be conspicuously posted at the point of entry into every

building where smoking is prohibited by Chapter 32 of the Sonoma County Code. The California Health and Safety Code (section 113978) also requires the posting of “No Smoking” signs in all food preparation areas, all retail food storage areas, and all food utensil washing areas. Note that Health and Safety Code section 113781 definition of food includes any beverage intended for human consumption.

18. A “Designated Smoking Area” may be established in unenclosed areas consistent with Sonoma County Code section 32-3. Designated Smoking Areas must be at least 25 feet away from any building or area where smoking is prohibited, must be conspicuously identified by signs as a smoking area, and shall be equipped with ash trays or ash cans.

SANITATION:

“The conditions below have been satisfied BY _____ DATE _____

Sonoma County Water Agency (Sonoma Water) operates and owns Airport/Larkfield/Wikiup Sanitation Zone (District). References to District employees are understood to be Sonoma Water employees.

19. The applicant shall comply with the INDUSTRIAL WASTE PLUMBING PLAN CHECK dated 14-Sep-2020, for building permit BLD19-8230: 1) “Special Conditions: ...Any changes in use or scope for the project will require a new WWDS to be submitted to Sonoma Water.” 2) “Additional Comments: Changes in use may also trigger additional pretreatment requirements and/or plumbing modifications including monitoring manholes, dual waste lines, or pretreatment equipment.”

Any additional sewer pre-treatment, separate process and domestic wastewater lines, and/or monitoring facilities are required by the Sonoma County Water Agency per this Survey, the Applicant shall comply with the requirements of the Survey prior to occupancy of the proposed warehouse and office.

20. The Applicant shall submit sewer plans to the Sanitation Section of PRMD for review and approval of the sanitary sewer design. Sewer plans shall be submitted electronically on-line at the Permit Sonoma website, and be printable to scale on 24 inch by 36 inch in size, and prepared by a licensed civil engineer registered in the State of California. Sanitary sewer facilities shall be designed and Sewer Plans prepared in accordance with SCWA Design and Construction Standards for Sanitation Facilities. The Applicant shall pay Plan Checking fees to the Sanitation Section of PRMD prior to the start of Sewer Plan Review.

Please note that review of the sanitary sewer design is a separate review from that of the buildings, drainage and frontage improvements, and shall be performed by the Sanitation Section of the Permit Sonoma under a separate permit.

21. Prior to the start of construction within the County road Right-of-Way of Aviation Boulevard, the Applicant shall have a licensed general contractor in possession of a valid Public Road bond obtain an Encroachment Permit from the Permit and Resource Management Department (PRMD).
22. At the time of sewer permit issuance, the Applicant shall provide the Sanitation Section of the Permit and Resource Management Department (PRMD) with data related to the floor area of the building, differentiating office, warehouse, etc., for the purpose of correctly calculating sewer use fees, as defined by Sonoma County Water Agency Sanitation Codes. Sewer use fees (including Connection and Annual Service fees) shall be paid prior to temporary occupancy, occupancy, and building permit final. No connection to sewer, building permit final, temporary occupancy, and building permit final shall be allowed until the sewer use fees are paid.

Sewer Use Fees, including connection and service fees, shall be paid for a minimum of 1.00 Equivalent Single-family Dwelling billing units (ESD’s) prior to connection to sewer, building

permit final, temporary occupancy, sewer permit final, and building permit final.

23. Sewer Use Fees for sewer service shall be calculated at the prevailing Sewer Connection and Annual Sewer Service Charge rates in effect at the time of sewer permit issuance.
24. All Sewer Use Fees per Airport/Larkfield/Wikiup Sanitation Zone Ordinances (latest revision) shall be paid to the Sanitation Section of the Sonoma County Permit and Resource Management Department (PRMD) prior to occupancy of the proposed warehouse and office.
25. The Applicant shall be responsible for the restoration of existing conditions including, but not limited to surfacing, landscaping, utilities and other public improvements that have been disturbed due to the construction of sanitary sewer facilities. Restoration shall be completed prior to the final of the sewer construction permit, unless otherwise specifically approved in advance by the Permit and Resource Management Department.
26. Prior to issuance of the building permit by the Sonoma County Permit and Resource Management Department (PRMD), the Applicant shall provide the Sanitation Section of PRMD with a statement from the Sonoma County Water Agency (SCWA), operators of the warehouse and office, addressing the current and future levels of collection and treatment capacity within the District. If it is determined by SCWA that a "Sewer Capacity Study" is warranted and required for the proposed project, the Applicant shall undertake to have this study prepared prior to issuance of the building permit.

If there is not adequate sewer capacity for the development, then the Applicant shall implement mitigation measures as directed by the SCWA, including but not limited to sewer main pipe upsizing, sealing of manholes and/or mains, and sewer flow reductions in other parts of the sanitary sewer system.

WATER (Town of Windsor):

27. The project will need to provide the following:
 - a. Install a separate water service, meter and backflow prevention device, meeting Town standards for domestic uses and landscape irrigation.
 - b. The project will be required to install a fire service with a Double Detector Check Valve (DDCV), meeting Town Standards for fire suppression purposes. All onsite hydrants and fire lines shall be privately owned and privately maintained.
 - c. An encroachment permit from the Town of Windsor will be required prior to performing work on any portion of the public water system.
 - d. Cross connections between the domestic, fire and irrigation systems will not be allowed.
 - e. A cross-connection specialist will need to submit written verification that no cross connections were observed prior to close-out of the Town's encroachment permit.
 - f. An additional separate grow meter will be required should the new warehouse be used for cannabis cultivation.
28. The landscape irrigation system shall consist of purple pipe to allow for future conversion to recycled water once recycled water becomes available.
29. All public water and recycled water mains must be located in the public right-of-way. Any and all rights-of-way, or if unavoidable, easement dedications, must be made by the property owner to the Town of Windsor and the Windsor Water District, at property owner's expense, prior to final acceptance of the project by the Town. The developer shall prepare all necessary legal descriptions and deeds.
30. All private potable water, fire service, and recycled water mains, laterals and appurtenances, must be located within the private property and clearly identified as "Private" on the design drawings.

31. All public water meters and backflow devices shall be located along a public street frontage, unless otherwise directed by the Water District Engineer, and must be contained within ft public right-of-way or public utility easement,
32. All public utilities shall be installed meeting Town standards and the separation requirements of the State of California Division of Drinking Water.
33. All landscaping, private and public, must be plumbed for recycled water and will need to be irrigated with Town of Windsor recycled water when it becomes available, unless otherwise approved by the Town Engineer.
34. The property owner must enter into and abide by all the terms of a Recycled Water User's Agreement with the Town of Windsor agreeing to use of recycled water when available. This Agreement must be signed by the property owner prior to approval of the improvement plans, issuance of a building permit, issuance of an encroachment permit or connection to the Town's recycled water system, or as otherwise approved by the Town Engineer. If recycled water is not available at the time of development, connection to the Town's recycled water system may be deferred until such time as recycled water becomes available to the project site. All deferred connections to the Town's recycled water system shall meet the requirements of the Town's Design and Construction Standards in force at that time of connection and as specified in the Recycled Water User's Agreement.
35. All recycled water mains, service laterals, plumbing, valves, pipes, appurtenances, irrigation parts, vaults, and boxes must be colored industry-standard purple. Recycled water notification signs shall be installed as directed by the Town Engineer. Recycled water spray, mists, and ponding must not be present in any designated eating area. All drinking fountains must be positioned or shielded to eliminate any exposure to recycled water sprays or mists.
36. Recycled water/potable water dual plumbing design and layout, construction-installation and final inspection review for individual lots or grouping of lots must be performed by an A WW A certified Cross Connection Specialist at the Property Owner's expense. All deficiencies cited by the Cross-Connection Specialist must be corrected by the property owner. Written reports of the Cross-Connection Specialist's findings must be submitted to and approved by the Public Works Director prior to acceptance.
37. Landscaping and irrigation shall meet the requirements of the Town's Water Efficient Landscape Ordinance (Ordinance No. 2015-73) as detailed in Section 12-3-900 of the Town's municipal code. WELO calculations shall be submitted using the commercial project specifications of the Town's MS Excel based WELO calculator found online at: <http://www.townofwindsor.com/documentcenter/view/14426>, which utilizes Windsor-specific parameters for evapotranspiration and rainfall. The WELO calculations, along with a landscape design plan, a completed hydrozone table, and an irrigation design plan shall be submitted for review and approval by the Town Engineer prior to improvement plan approval.
38. Single pass evaporative cooling systems for air conditioning shall not be permitted.
39. The owner shall fill out a Town of Windsor/Windsor Water District Water Permit Application Form for a specific use and/or type of business. For high water users (such as breweries, distilleries, wineries, cannabis growers, wet manufacturing, food processing, etc.), the owner shall provide an engineering report with the water use form that provides information pertaining to best available technology proposed to conserve water use and maximize reuse of water. The form and report shall be submitted to the Public Works Director/Windsor Water District Engineer for review and acceptance prior to the Town and District agreeing to serve water to the owner's property.
40. The owner shall fill out a Windsor Water District Property Owner Authorization form and return it with the water permit application.

GRADING AND STORM WATER:

"The conditions below have been satisfied BY _____ DATE _____

41. Grading and/or building permits require review and approval by the Grading & Storm Water Section of the Permit and Resource Management Department (Permit Sonoma) prior to issuance. Grading permit applications shall abide by all applicable standards and provisions of the Sonoma County Code and all other relevant laws and regulations.
42. A drainage report for the proposed project shall be prepared by a civil engineer, currently registered in the State of California, be submitted with the grading and/or building permit application and be subject to review and approval by the Grading & Storm Water Section of the Permit and Resource Management Department (Permit Sonoma) . The drainage report shall include, at a minimum, a project narrative, on- and off-site hydrology maps, hydrologic calculations, hydraulic calculations, pre- and post-development analysis for all relevant existing and proposed drainage facilities. The drainage report shall abide by and contain all applicable items in the Drainage Report Required Contents (DRN-006) handout.
43. The proposed project is subject to storm water Low Impact Development (LID) regulations. Post-construction storm water LID best management practices (BMP's) to mitigate impacts to the quality and quantity of storm water discharges from the project site shall be incorporated into the drainage design of the project. A final Storm Water Low Impact Development Submittal (SW LIDS), based upon the approved initial SW LIDS dated 12/20/2019, shall be submitted with the grading and/or building permit application and be subject to review and approval by the Grading & Storm Water Section of the Permit and Resource Management Department (Permit Sonoma) prior to the issuance of any grading or building permits. LID BMP's must be installed per approved plans and specifications, and working properly prior to finaling the grading permit and associated building permits.
44. Landowner shall submit to Permit and Resource Management Department (Permit Sonoma) annually the results of the Storm Water BMP inspection. Inspection shall include but is not limited to the Inspection and Maintenance Checklists, photo evidence of BMP existing conditions, and a report of any maintenance activity, remediation, or replacement of BMP features.
45. Drainage improvements shall be designed by a civil engineer, currently registered in the State of California, and in accordance with the Sonoma County Water Agency Flood Control Design Criteria. Drainage improvements shall be shown on the grading/site plans and be submitted to the Grading & Storm Water Section of the Permit and Resource Management Department (Permit Sonoma) for review and approval. Drainage improvements shall maintain off-site natural drainage patterns, limit post-development storm water quantities and pollutant discharges in compliance with Permit and Resource Management Department's best management practices guide, and shall abide by all applicable standards and provisions of the Sonoma County Code and all other relevant laws and regulations. Existing drainage patterns shall be maintained, to the maximum extent practicable, to not adversely impact adjacent properties or drainage systems. Proposed drainage improvements shall not adversely impact adjacent properties or drainage systems.
46. The applicant shall provide grading plans, prepared by a civil engineer currently registered in the State of California, which clearly indicate the nature and extent of the work proposed and include all existing and proposed land features, elevations, roads, driveways, buildings, limits of grading, limits of disturbed area/total work, adequate grading cross sections and drainage facilities such as swales, channels, closed conduits, or drainage structures. The grading plans shall abide by and contain all applicable items from the Grading Permit Required Application Contents (GRD-004) handout.

47. As part of the grading plans, the applicant shall include an erosion prevention/sediment control plan which clearly shows best management practices to be implemented, limits of disturbed areas/total work, vegetated areas to be preserved, pertinent details, notes, and specifications to prevent damages or minimize adverse impacts to the surrounding properties and the environment. Tracking of soil or construction debris into the public right-of-way shall be prohibited. Runoff containing concrete waste or by-products shall not be allowed to drain to the storm drain system, waterway(s), or adjacent lands. The erosion prevention/sediment control plan shall abide by and contain all applicable items in the Grading Permit Required Application Contents (GRD-004) handout.
48. Residue or polluted runoff from the production areas/activities shall not be allowed to drain directly to the storm drain system, waterway(s) or adjacent lands. Production areas shall be covered or drain directly to a proper waste disposal system. No diversion valves shall be allowed.
49. Runoff from waste receptacles or outside washing areas shall not be allowed to drain directly to the storm drain system, waterway(s) or adjacent lands. Areas used for waste receptacles and outside washing areas shall be covered or separated from the rest of the project site by grade breaks that prevent storm water run-on. Any surface water flow from a waste receptacle or outside washing area shall not be permitted to enter the storm drain system without receiving appropriate treatment.
50. Drainage easements are required when conveying storm water runoff through any neighboring properties. Drainage easements shall be clearly shown and noted on the grading/site plans. The applicant shall be responsible for obtaining or creating drainage easements necessary for the proposed project prior to grading or building permit issuance. Any proposed drainage easements shall be private easements unless otherwise approved by the Department of Transportation and Public Works.
51. If the cumulative land disturbance of the project is equal to or greater than one (1) acre, then the project is subject to National Pollutant Discharge Elimination System (NPDES) requirements and must obtain coverage under the State Water Resource Control Board's General Construction Permit (General Permit). Documentation of coverage under the General Permit must be submitted to the Grading & Storm Water Section of the Permit and Resource Management Department prior to issuance of any grading permit for the proposed project.

TRANSPORTATION AND PUBLIC WORKS:

"The conditions below have been satisfied BY _____ DATE _____

52. The Applicant, his or her personal representatives, and project consultants are advised that the issuance of building permits is subject to the payment of a development fee (Traffic Mitigation Fee) as required by Section 26, Article 98 of the Sonoma County Code. The fee is computed multiplying project Average Daily Traffic (ADT) by the commercial fee in effect at the time of permit issuance. Evidence of payment shall be submitted to the Land Development Section of the Permit and Resource Management Department prior to clearance of these conditions.

FIRE AND EMERGENCY SERVICES:

"The conditions below have been satisfied BY _____ DATE _____

53. The subject property (or properties) must be in full compliance with Building Code regulations, Fire Code Regulations and Hazardous Materials regulations prior to occupancy of the buildings and use of the property without written approval by the fire code official.
 - a. A fire inspection shall be conducted by the Sonoma County Fire Prevention Division to determine if the subject property (or properties) is currently in full compliance with

- applicable Building Code regulations, Fire Code Regulations and Hazardous Materials regulations.
- b. The Sonoma County Fire Prevention Division may charge and collect a fee for the inspection in an amount, as determined by the county, sufficient to pay its costs of that inspection.
 - c. The Building (s) shall be fire sprinklered and contain a fire alarm system consistence with the occupancy type.
54. Applicable Fire Code operational permits shall be obtained prior to the initiation of any activity where an operational permit is required by the California Fire Code as adopted and amended by Sonoma County Code.
- a. Fire protection system
 - b. Hazardous material: storing, transporting on site, dispensing, using or handling hazardous materials
 - c. High-piled storage exceeding 500 square feet
 - d. Hot work operations
 - e. LP-gas Storage and use over 500 gallons water capacity and Operation of cargo tankers that transport LP-gas
 - f. Miscellaneous combustible storage in excess of 2,500 cubic feet
 - g. Private fire hydrant: service, use or operation
55. An annual fire safety inspection is required for any facility requiring a Fire Code Operational Permit. The county or fire district which inspects the facility may charge and collect a fee for the inspection from the owner of the facility in an amount, as determined by the county or fire district, sufficient to pay its costs of that inspection.
56. The facility operator shall develop an emergency response plan consistent with Chapter 4 of the 2013 California Fire Code as adopted and amended by Sonoma County Code. Fire safety plans, emergency procedures, and employee training programs shall be approved by the fire code official.
57. To facilitate locating an emergency and to avoid delays in response; all existing and newly constructed or approved roadways and buildings whether public or private shall provide for safe access for emergency fire apparatus and civilian evacuation concurrently, and shall provide unobstructed traffic circulation during an emergency, and shall be constructed and maintained as required by Sonoma County Fire Safe Standards and the California Fire Code, as adopted and amended by Sonoma County Code.
58. Emergency water supply for fire protection shall be available and accessible in locations, quantities and delivery rates as specified in the California Fire Code as adopted and amended by Sonoma County Code.
- a. Fire hydrants shall be installed as required by the California Fire Code as adopted and amended by Sonoma County Code.
 - b. Emergency water supply for fire suppression may be provided in a naturally occurring or manmade containment structure for projects not served by a municipal water system, as long as the specified quantity and delivery rate is immediately provided.
59. Local Responsibility Area (LRA). Compliance with the general defensible space clearances per CCR Title 19 Div. 1, Chapter 1, Subchapter 1 §3.07.
60. Prior to occupancy, written approval that the required improvements have been installed shall be provided to Permit Sonoma from the County Fire Marshal/Local Fire Protection District.

PLANNING:

“The conditions below have been satisfied BY _____ DATE _____

61. The Use Permit (UPE19-0081) shall supersede all prior Use Permits, upon implementation or when all the pre-operational conditions have been met and this Use Permit is vested.

PRIOR TO PERMIT ISSUANCE

62. **Mitigation Measure AIR-1:** The following dust and air quality control measures shall be included in the project:
- a. Water or alternative dust control method shall be sprayed to control dust on construction areas, soil stockpiles, and staging areas during construction as directed by the County.
 - b. Trucks hauling soil, sand, and other loose materials over public roads shall cover the loads, or shall keep the loads at least two feet below the level of the sides of the container, or shall wet the load sufficiently to prevent dust emissions.
 - c. Vehicle speeds on unpaved areas shall be limited to 15 miles per hour.
 - d. Final surfacing (i.e., pavement or concrete, gravel, landscaping) shall be completed as soon as possible after earthwork is finished, unless seeding or soil binders are used.
 - e. Idling time of diesel-powered construction equipment shall be limited to five minutes. Signs shall be posted reminding workers of this idling restriction at all access points and equipment staging areas during construction of the proposed project.
 - f. All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications and shall have a CARB-certified visible emissions evaluator check equipment prior to use at the site.
 - g. Trackout shall not be allowed at any active exit from the project site onto an adjacent paved public roadway or shoulder of a paved public roadway that exceeds cumulative 25 linear feet and creates fugitive dust visible emissions without cleaning up such trackout within 4 hours of when the Construction Coordinator identifies such excessive trackout, and shall not allow more than 1 quart of trackout to remain on the adjacent paved public roadway or the paved shoulder of the paved public roadway at the end of any workday.
 - h. Visible emissions of fugitive dust shall not be allowed during cleanup of any trackout that exceeds 20 percent opacity as determined by the Environmental Protection Agency in Method 203B - Opacity Determination for Time-Exception Regulations (August 2017).
 - i. Trackout is defined by BAAQMD in Regulation 6, Rule 6: Prohibition of Trackout (August 2018) as any sand, soil, dirt, bulk materials or other solid particles from a site that adhere to or agglomerate on the exterior surfaces of vehicles (including tires), and subsequently fall or are dislodged onto a paved public roadway or the paved shoulder of a paved public roadway on the path that vehicles follow at any exit and extending 50 feet out onto the paved public roadway beyond the boundary of the site. Material that has collected on the roadway from erosion is not trackout.

Monitoring AIR-1: Permit Sonoma staff shall verify that the AIR-1 measures are included on all site alteration, grading, building or improvement plans prior to issuance of grading or building permits.

63. **Mitigation Measure BIO-1:** If initial ground disturbance occurs during the flowering periods for bent-flowered fiddleneck (March-June), two-fork clover (April-June), or congested-headed hayfield tarplant (April-November), a qualified biologist shall conduct a pre-construction survey of the disturbance area prior to construction activities. Surveys shall be either focused or protocol-level surveys and follow methodologies outlined in relevant agency protocols. If special-status plants are observed, their locations shall be mapped and CDFW shall be contacted to determine the appropriate mitigation measure to avoid impacts on the species, including avoidance, creation of buffers, transplantation, or otherwise off-site mitigation.

Monitoring BIO-1: Prior to issuance of any grading permit(s) and through completion of initial site disturbance, the County shall review the results of all pre-construction surveys and any measures recommended by the biologist or CDFW to protect sensitive habitat or species. All measures shall be noted on the final project plans.

64. **Mitigation Measure BIO-2:** If initial ground disturbance or tree removal occurs during the

breeding season for nesting birds (February 1 through August 31), a qualified biologist shall conduct a breeding bird survey no more than 7 days prior to ground disturbance to determine if any birds are nesting in underground burrows or dens, or in trees on or adjacent to the project site. If active nests are found close enough to the project site to affect breeding success, the biologist shall establish an appropriate exclusion zone around the nest. This exclusion zone may be modified depending on the species, nest location, and existing visual buffers, but typically would entail a minimum of 500 feet for raptor species and 300 feet for other migratory species. Once all young have become independent of the nest, vegetation removal and grading may take place in the former exclusion zone. If initial ground disturbance is delayed or there is a break in project activities of more than 7 days within the bird-nesting season, then a follow-up nesting bird survey shall be performed to ensure no nests have been established in the interim.

Monitoring BIO-2: Prior to issuance of any grading permit(s) and through completion of initial site disturbance, the County shall review the results of all pre-construction surveys and any measures recommended by the biologist or CDFW to protect sensitive habitat or species. All measures shall be noted on the final project plans.

65. **Mitigation Measure BIO-3:** Prior to issuance of building or site development permits, the applicant is required to provide mitigation for impacts to potential CTS habitat, consistent with requirements of the Santa Rosa Plain Conservation Strategy and the 2020 Programmatic Biological Opinion. All CTS mitigation will be provided at an off-site location and will consist of the purchase of CTS credits from an approved mitigation bank. The appropriate mitigation ratio area shall be no less than 0.2:1 unless the applicant is able to obtain a “no effect” determination or similar clearance by the USFWS.

Monitoring BIO-3: Permit Sonoma staff shall withhold issuance of site development permits until verification is received indicating that CTS mitigation has been completed.

66. **Mitigation Measure CUL-1:** All building and/or grading permits shall have the following note printed on grading or earthwork plan sheets:

“If paleontological resources or prehistoric, historic or tribal cultural resources are encountered during ground-disturbing work, all work in the immediate vicinity shall be halted and the operator must immediately notify the Permit and Resource Management Department (PRMD) – Project Review staff of the find. The operator shall be responsible for the cost to have a qualified paleontologist, archaeologist or tribal cultural resource specialist under contract to evaluate the find and make recommendations to protect the resource in a report to PRMD. Paleontological resources include fossils of animals, plants or other organisms. Prehistoric resources include humanly modified stone, shell, or bones, hearths, firepits, obsidian and chert flaked-stone tools (e.g., projectile points, knives, choppers), midden (culturally darkened soil containing heat-affected rock, artifacts, animal bone, or shellfish remains), stone milling equipment, such as mortars and pestles, and certain sites features, places, cultural landscapes, sacred places and objects with cultural value to a California Native American tribe. Historic resources include all by-products of human use greater than fifty (50) years of age including, backfilled privies, wells, and refuse pits; concrete, stone, or wood structural elements or foundations; and concentrations of metal, glass, and ceramic refuse.

If human remains are encountered, work in the immediate vicinity shall be halted and the operator shall notify PRMD and the Sonoma County Coroner immediately. At the same time, the operator shall be responsible for the cost to have a qualified archaeologist under contract to evaluate the discovery. If the human remains are determined to be of Native American origin, the Coroner must notify the Native American Heritage Commission within 24 hours of this identification so that a Most Likely Descendant can be designated and the appropriate measures implemented in compliance with the California Government Code and Public Resources Code.”

Monitoring CUL-1: Building/grading permits shall not be approved for issuance by Permit Sonoma staff until the above notes are printed on the building, grading, and improvement plans.

67. **Mitigation GHG-1:** The applicant shall submit a Greenhouse Gas Reduction Plan for PRMD

review and approval that defines measures to reduce greenhouse gas emissions in the design, construction, and long-term operations of the project. The Greenhouse Gas Reduction Plan shall include all reasonably feasible measures to reduce greenhouse gas emissions to the maximum extent feasible. Measures that must be evaluated include but are not limited to best available conservation technologies for all energy and water uses, installation of renewable energy facilities to meet demand on-site, provisions of electric vehicle charging stations, bicycle facilities including secure bike parking, and lockers and showers for employees, employing best management practices for carbon sequestration, such as no till soils, reduced use of fertilizers, etc.

Monitoring GHG-1: Permit Sonoma staff shall ensure that the methods selected in the Greenhouse Gas Emissions Reduction Plan are listed on all site alteration, grading, building or improvement plans prior to issuance of grading or building permits. Building/grading permits shall not be approved for issuance by Project Review Staff until the Greenhouse Gas Reduction Plan has been approved and incorporated into the design and construction documents for the project.

68. **Mitigation Measure NOISE-1:** All plans and specifications or construction plans shall include the following notes:

- a. All internal combustion engines used during construction of this project will be operated with mufflers that meet the requirements of the State Resources Code, and, where applicable, the Vehicle Code. Equipment shall be properly maintained and turned off when not in use.
- b. Except for actions taken to prevent an emergency, or to deal with an existing emergency, all construction activities shall be restricted to the hours of 7:00 a.m. and 7:00 p.m. (use this if no nearby receptors, or 5:00 pm if nearby receptors) on weekdays and 9:00 a.m. and 7:00 p.m. (same note as above) on weekends and holidays. If work outside the times specified above becomes necessary, the applicant shall notify the Permit Sonoma Project Review Division as soon as practical.
- c. There will be no start up of machines nor equipment prior to 7:00 a.m, Monday through Friday or 9:00 am on weekends and holidays; no delivery of materials or equipment prior to 7:00 a.m nor past 7:00 p.m, (same note as above) Monday through Friday or prior to 9:00 a.m. nor past 7:00 p.m. on weekends and holidays and no servicing of equipment past 7:00 p.m., Monday through Friday, or weekends and holidays. A sign(s) shall be posted on the site regarding the allowable hours of construction, and including the developer- and contractors mobile phone number for public contact 24 hours a day or during the hours outside of the restricted hours.
- d. Pile driving activities shall be limited to 7:30 a.m. to 7:00 p.m. weekdays only (same note as above).
- e. Construction maintenance, storage and staging areas for construction equipment shall avoid proximity to residential areas to the maximum extent practicable. Stationary construction equipment, such as compressors, mixers, etc., shall be placed away from residential areas and/or provided with acoustical shielding. Quiet construction equipment shall be used when possible.
- f. The developer shall designate a Project Manager with authority to implement the mitigation prior to issuance of a building/grading permit. The Project Managers 24-hour mobile phone number shall be conspicuously posted at the construction site. The Project Manager shall determine the cause of noise complaints (e.g. starting too early, faulty muffler, etc.) and shall take prompt action to correct the problem.

Monitoring NOISE-1: Permit Sonoma Project Review Division staff shall ensure that the measures are listed on all site alteration, grading, building or improvement plans, prior to issuance of grading or building permits. Permit Sonoma staff shall inspect the site prior to construction to assure that the signs are in place and the applicable phone numbers are correct. Any noise complaints will be investigated by Permit Sonoma staff. If violations are found, Permit Sonoma shall seek voluntary compliance from the permit holder, or may require a noise consultant to evaluate the problem and recommend corrective actions, and thereafter may initiate an enforcement action and/or revocation or modification proceedings, as appropriate.

69. At the time of submitting a building permit application, the applicant shall submit to PRMD a Condition Compliance Review fee deposit (amount to be determined consistent with the ordinance in effect at the time). In addition, the applicant shall be responsible for payment of any additional compliance review fees that exceed the initial deposit (based upon hours of staff time worked) prior to final inspection being granted.
70. The applicant shall include these Conditions of Approval on separate sheets of plan sets to be submitted for building and grading permit applications.
71. All grading and development on site shall be done in compliance with the County Tree Protection Ordinance, including protection of trees during construction with a chain link fence at the drip-line, and replacement of damaged or removed trees. The project's grading and landscape plans shall detail all tree protection implementation measures.
- PRMD shall not sign off the grading or building permit for issuance until the project grading and landscape construction documents clearly show all tree protection measures (as required in the County Tree Protection Ordinance). PRMD shall not sign off the grading or building permit for occupancy until a site inspection has been conducted, and the applicant has provided written verification from the project's landscape architect or contractor, that the tree protection measures were complied with.
72. A Water Conservation Plan shall be submitted for all buildings and landscaping prior to building permit issuance, subject to Permit Sonoma review and approval. The Water Conservation Plan shall include all reasonably feasible measures to reduce water demand to the maximum extent feasible and enhance water resource recovery to maintain sustainable water supplies. Measures that must be evaluated include: installation of low-flow fixtures, best available conservation technologies for all water uses, rainwater and stormwater collection systems and graywater reuse. The measures in the plan shall be implemented by the applicant and verified by Permit Sonoma staff prior to Certificate of Occupancy or operation of the use.

OPERATIONAL

73. All new structures, lighting and signs shall require final design review by (the Project Review Division or Design Review Committee) prior to issuance of building permits. All exterior finishes shall be of non-reflective materials and colors.
74. Exterior lighting shall be low mounted, downward casting and fully shielded to prevent glare. Lighting shall not wash out structures or any portions of the site. Light fixtures shall not be located at the periphery of the property and shall not spill over onto adjacent properties or into the night sky. Flood lights are not permitted. All parking lot and street lights shall be full cut-off fixtures. Lighting shall shut off automatically after closing and security lighting shall be motion sensor activated.
75. Parking lot fixtures shall not exceed 20 feet in height. All parking lot and/or street light fixtures shall use full cut-off fixtures.
76. All exterior fixtures shall be limited to lamps (light bulbs not exceeding 100 watts).
77. The operator of the establishment shall take all reasonable steps, including contacting law enforcement in a timely manner, to prevent customers or other persons from engaging in objectionable activities on the premises, parking areas under the control of the operator, and other public or quasi-public areas within site of the premises during business hours.
78. Trash, Litter, and Graffiti.
- a. At least twice a week, the operator of the establishment shall remove trash, litter, and debris from the sidewalks adjoining the premises plus 10 feet beyond property lines as well as any parking lots under the control of the operator.

- b. The operator of the establishment shall remove all graffiti from the premises and parking lots under the control of the operator within 72 hours of its application.
- c. Trash can only be disposed in exterior dumpsters between the hours of 8:00 a.m. and 8:00 p.m.